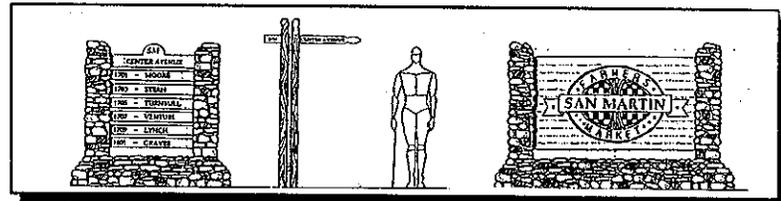
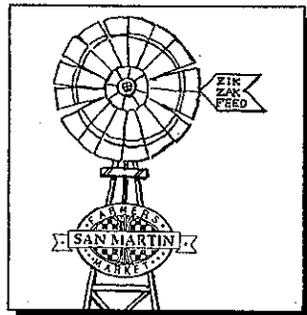
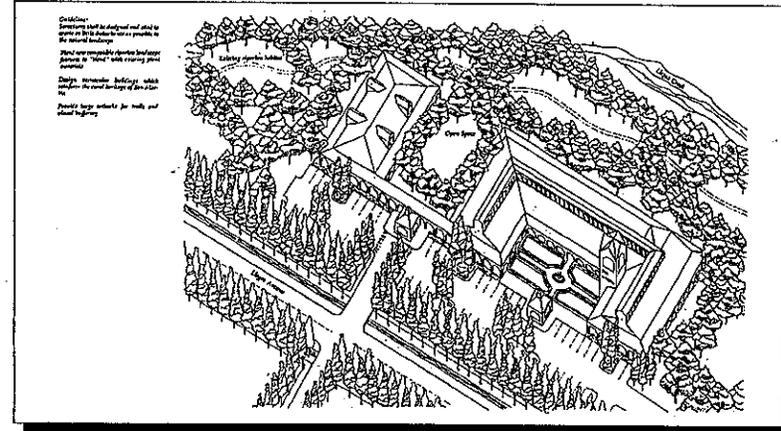
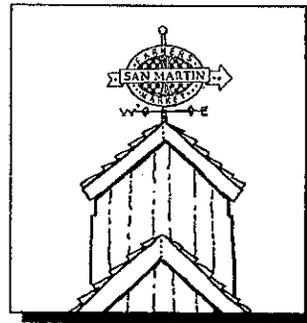
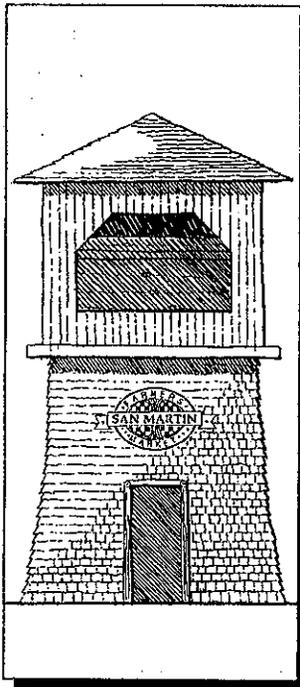
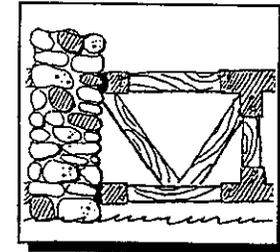
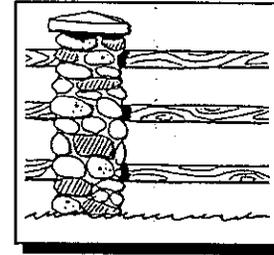
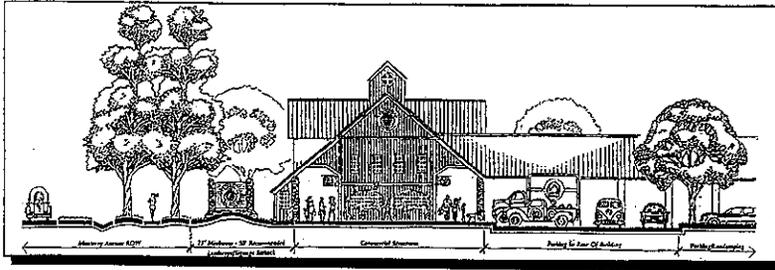


# San Martin Integrated Design Plan



County Of Santa Clara

urban  
design  
studio

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County of Santa Clara

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# SAN MARTIN INTEGRATED DESIGN PLAN AND GUIDELINES

Adopted by the Board of Supervisors \_\_\_\_\_

Prepared by:

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# SAN MARTIN INTEGRATED DESIGN PLAN AND GUIDELINES

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# I. INTRODUCTION

## A. Background

San Martin is an unincorporated, predominantly rural residential community located in South Santa Clara County between Morgan Hill and Gilroy. The San Martin planning area is approximately 6,300 acres and is bounded by Maple Avenue on the north and Fitzgerald-Masten Avenues on the South (see Figure 1).

The San Martin area is divided approximately in half by the South Valley Freeway which runs in a north-south direction. The area west of the freeway is more intensively developed and supports all of the existing commercial and industrial uses in San Martin. The area east of the freeway, which is the more rural part of the community, is characterized by low to medium-density, single family residences and various agricultural land uses.

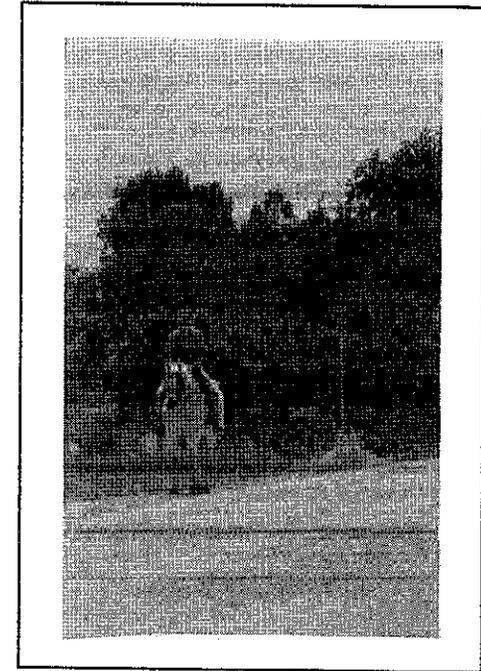
The "San Martin Village" is centered at the intersection of Monterey Road and San Martin Avenue. This community core consists of single-family lots and most of the commercial and industrial uses within the San Martin area. Approximately 300 acres in size, the community core is characterized by a predominance of small lots, a village atmosphere, and higher population density than the remainder of San Martin.

## B. Purpose Of The San Martin Community Design Plan

An increasing level of development pressure is being placed upon the San Martin area. The area is situated between the urbanizing communities of Gilroy and Morgan Hill. In a sense, San Martin, with its rural atmosphere can be looked upon as a very independent agrarian community characterized by oak studded hills, creeks, agricultural orchards and large rural oriented residential parcels.

The County of Santa Clara endorses the desire of San Martin residents to retain their rural lifestyle in contrast to the urban areas of the region which will become intensely developed in future years. In order to protect the rural lifestyle in San Martin in the face of growing development pressure, the County endorsed the preparation of a study which would develop a set of integrated development/design guidelines to achieve the desired rural character for San Martin.

The design guidelines in this design plan are intended to inform and guide public and private property development in San Martin so that the form and character of the overall community is protected and enhanced. This Plan is recommended to be provided to developers, homeowners, State and local agencies, and the County so that early design decisions can be made that are consistent with the Plan.

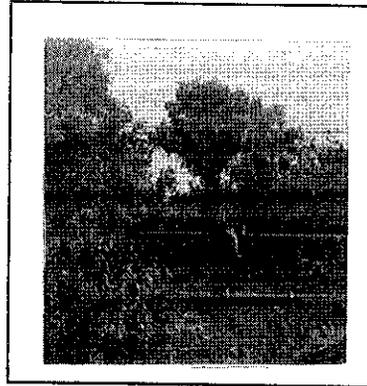


*Rural Lifestyle*

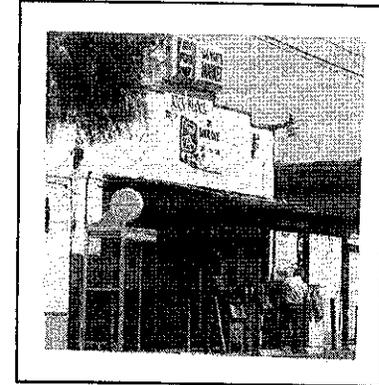
## C. Plan Components

The design elements contained in this Plan consist of four components. These are:

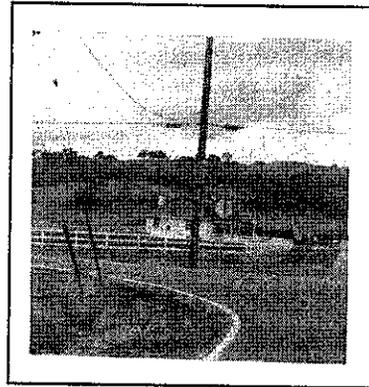
1. Flood Control/Drainage Design Concepts
2. Circulation and County Roads Conceptual Design Standards
3. Rural Residential Design Concepts
4. Non-Residential Design Guidelines



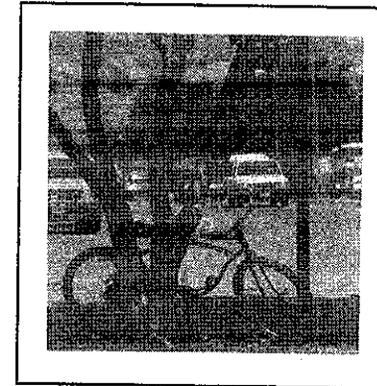
*Bridge Over Llagas Creek*



*San Martin Avenue*



*Rural Residential Area*



*Village Core Commercial*

## D. Community Visioning Process

In order to gain a firm understanding of what residents of San Martin perceived as the most important issues associated with community design and maintaining "ruralness," the firm of Phillips Brandt Reddick (PBR), Urban Design Studio (UDS) and County staff conducted an intensive two day visioning exercise. Utilizing the four components of the Plan, flood control, circulation/roads, rural residential design and non-residential design, members of the community in attendance were asked to define specific issues associated with each component topic. The various issues include the following:

### Flood Control/Drainage

- ▲ Trails along Llagas Creek
- ▲ Development interface with Llagas Creek
- ▲ Localized flooding problems
- ▲ Consistency/inconsistency of roadside drainage ditch design

### Circulation and County Roads

- ▲ Development interface with scenic highways
- ▲ Excessive vehicular speeds on local roads
- ▲ Safety associated with pedestrian/equestrian crossing of county roads
- ▲ Accommodating various trails/buffers along country roads
- ▲ Avoidance of "urban" construction standards and widths on local country roads

### Rural Residential Design Concepts

- ▲ Rural 5 acre lot subdivision practices and clustering options
- ▲ Development guidelines/ideas for the 1 acre and 5 acre parcels
- ▲ Protection of existing rural qualities and the natural environment
- ▲ Setback and landscape standards for development abutting Monterey Road and Masten Avenue

- ▲ Compatible architectural style ideas

### Non-Residential Design Guidelines

- ▲ Attractive commercial design in the village core
- ▲ Attractive commercial development at San Martin Avenue/South Valley Freeway interchange
- ▲ Screening of parking and loading areas

These were the issues identified by the residents of San Martin as having the most importance. The San Martin Community Design Plan graphically addresses these and other design issues.

## E. Use Of The Guidelines

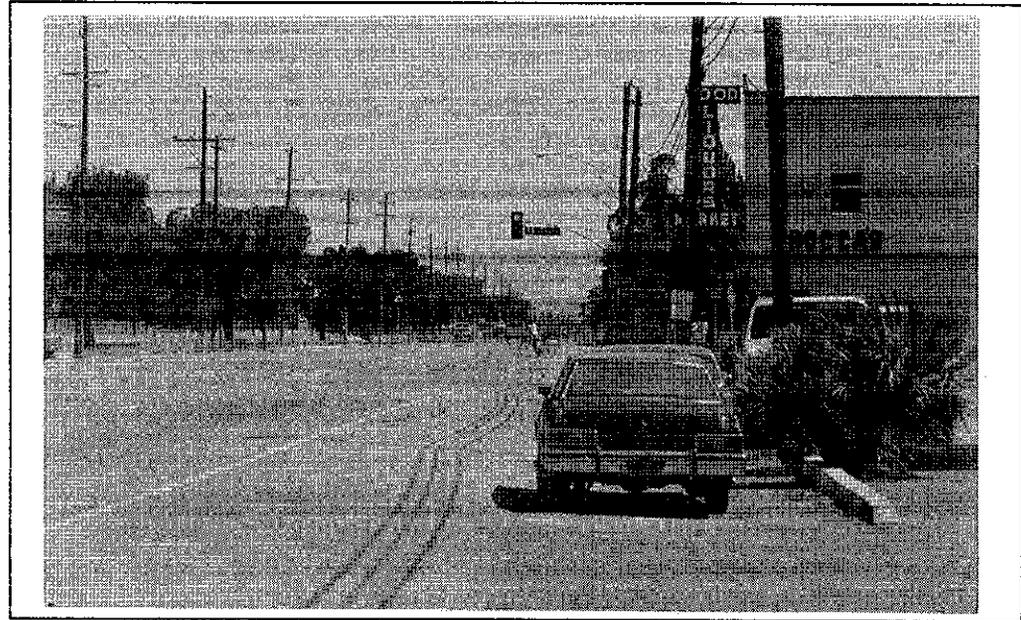
The guidelines contained in the San Martin Community Design Plan should be considered when designing all new non-residential projects. Roadway and flood control improvement projects in the San Martin Planning Area should be based on standards contained in the guidelines. In some cases, the guidelines may also be applied to residential development located in the village core, which requires a Use Permit. The guidelines provided in this document are designed to supplement the *general* Architecture and Site Approval (ASA) guidelines while "tailoring" *specific* guidelines to the unique conditions found within San Martin.

The Board of Supervisors, Planning Commission, ASA Committee, Secretary of the Planning Commission, and the Santa Clara Valley Water District (SCVWD) may use these guidelines to develop conditions of approval for projects and to establish mitigations for any potential

significant adverse environmental impact associated with the proposed development.

Standards proposed in this Plan may be used by the County to develop alternative road standards for the San Martin area.

The purpose of the design guidelines is to provide decisionmakers with the criteria necessary to make informed decisions related to aesthetic concerns and physical improvements. However, not all of the guidelines may apply to a particular project.



*Intersection of Monterey Street and San Martin Avenue*



## II. NON-RESIDENTIAL DESIGN GUIDELINES COMPONENT

### A. Intent

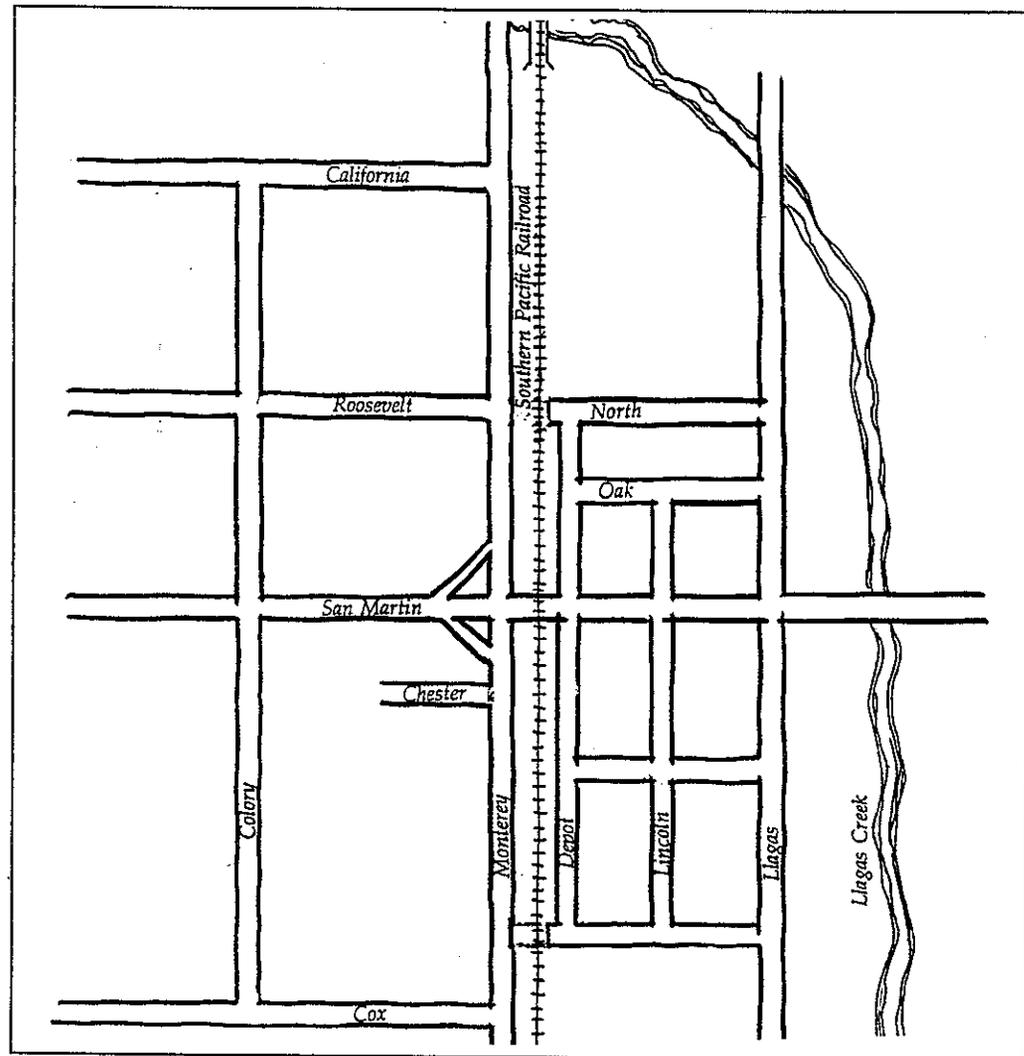
The San Martin Non-Residential Development Design Guidelines were prepared in response to community concerns which prompted a General Plan amendment, approved by the County Board of Supervisor on December 13, 1988. The General Plan amendment added a policy to the General Policies section of the San Martin Land Use Policies. The primary objective of this policy was to insure that new and expanded non-residential development, primarily in the village core of San Martin, be of a nature, design and intensity which reinforced the predominantly rural character of San Martin, thus moving to unify and improve the overall aesthetic appearance of the area.

These guidelines serve to encourage excellence of design and insure that reasonable steps be taken to mitigate potential adverse impacts on the natural environment.

## B. General Character

Although these design guidelines apply to all new non-residential development in the San Martin Planning Area, three general areas were identified as the focus of the guidelines. The first and most distinctive area, referred to as the "village core," is located around the intersection of San Martin Avenue and Monterey Road. The design objective for the "village core" is to create an intimate, attractive pedestrian oriented area. Design features which might accomplish this objective include small scale pedestrian oriented structures, narrow setbacks, landscaping to create intimate "people places," restricted use of signage and the use of other appropriate design components to create a Main Street atmosphere.

Vital villages have certain unifying characteristics. The best village areas have a combination of town and country qualities that people find satisfying. Good villages serve not only the business and commercial needs of the city, but also as an expression of rural living; the cultural hub and psychological center of the region. People are attracted to them as special areas of the region because of their mixture of stores, offices, restaurants and public buildings.

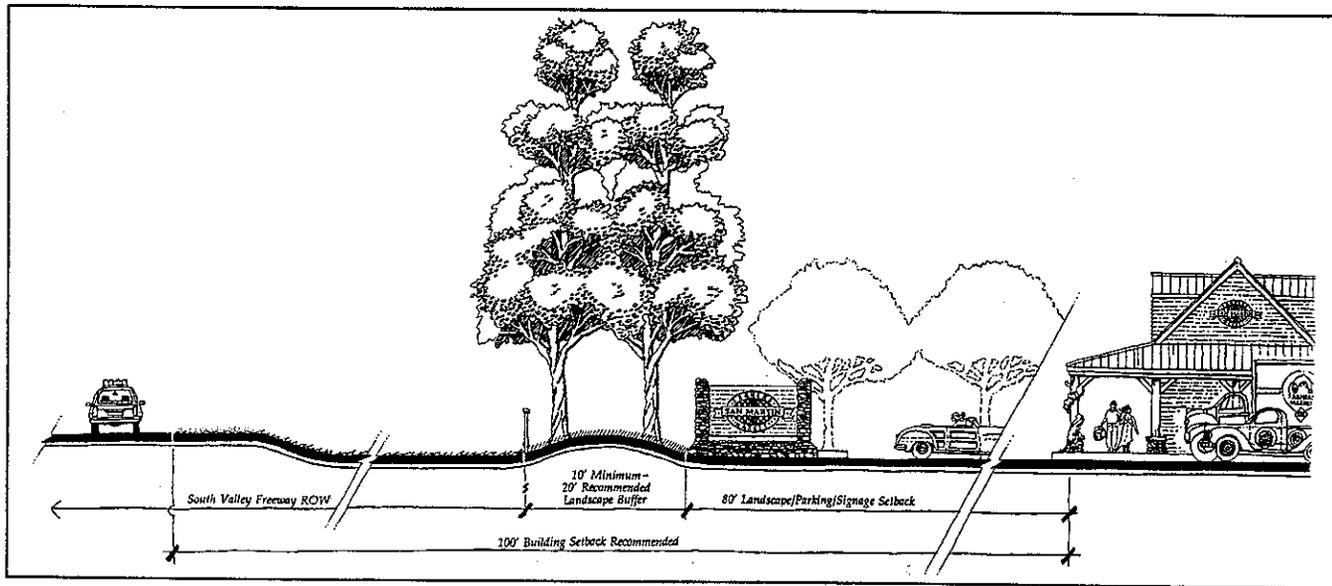


Good villages are stimulated in their density and diversity of activities and their variety of building ages, sizes and styles. They are interesting places for people to be because of their unique combination of public spaces, architecture and human activity.

The second area is the portion of Monterey Road which runs south from the "village core," and the small portion of San Martin Avenue which runs east between the San Martin Post Office and the San Martin Avenue/South Valley Freeway Interchange.

It is important to recognize that there are, and will continue to be, uses in these areas which are not pedestrian oriented (e.g. building supply stores, auto dismantlers, etc.) and which require different design treatment. These uses, although vehicle oriented, should still be consistent with the community's general rural oriented objectives for San Martin. Design features to incorporate into these projects could include: screened parking and loading areas, landscape park strips, small to medium scaled buildings, etc.

The third area consists primarily of the South Valley Freeway interchanges at San Martin and Masten Avenues and the properties that lie adjacent to the scenic South Valley Freeway (Highway 101). The design objective for this area is to create a vehicular oriented setting, while still maintaining and enhancing the value of the scenic highway. Design features which might accomplish this objective include clustering of structures, generous setbacks, strict signage controls, and the extensive use of landscaping around the structures and to screen loading and parking areas.

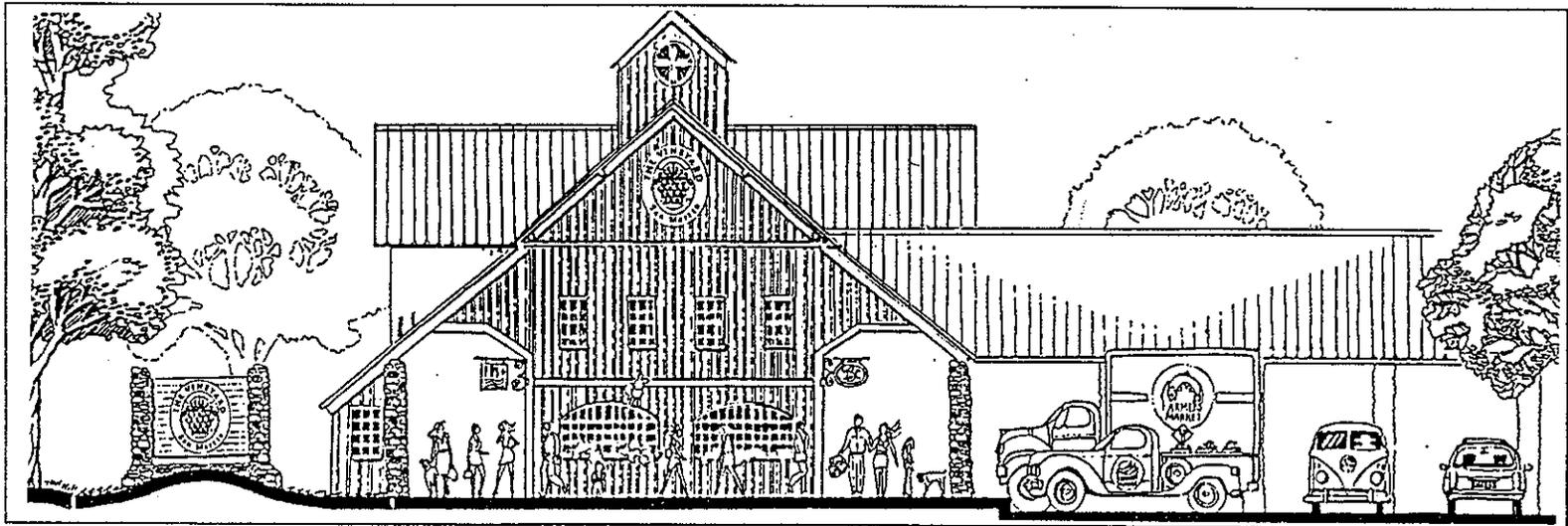


▲ Building setbacks are measured from freeway paved surface

Development Interface With Freeway

## C. Architecture

1. The proposed non-residential structure(s) shall enhance, through excellence of design, the San Martin community as a whole, as well as the immediate neighborhood.
  - a. All aspects of the non-residential building shall comply with the design guidelines. Equal attention shall be given to the structure as viewed from the street, sides and rear; and especially from the South Valley Freeway, which is designated by the County as a Scenic Highway.
  - b. Appropriate architectural styles which promote the rural character of San Martin, and which are compatible with the surrounding neighborhood shall be required. Examples include, but are not limited to: California Ranch, Western and Victorian style. Residential dwellings located in the commercial/industrial use permit area are encouraged to follow the Rural Characteristics Guidelines found in Section IV-F of this document.
2. The general architectural style of the non-residential structure(s) shall be compatible with the architecture of the neighboring uses and surrounding community in such a way as to enhance the desired visual character of San Martin.
  - a.
  - b.



*Appropriate Commercial Architecture*

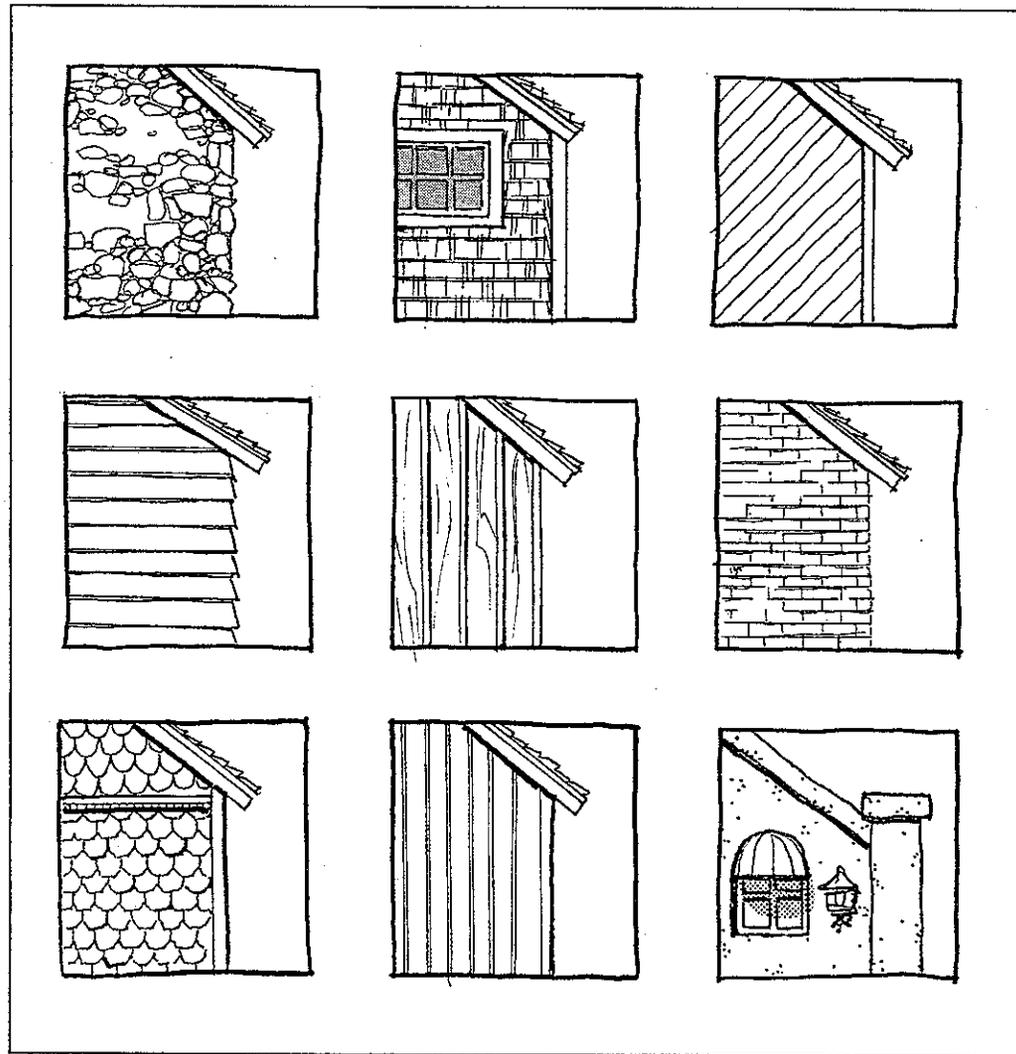
3. Buildings and other non-residential structures shall create an attractive exterior form by using variation in the arrangement of colors, textures and materials.

4. Materials and colors shall be appropriate to ensure compatibility with the natural setting, the surrounding neighborhood and the intent of these design guidelines.

a. Natural looking materials such as adobe, wood, stone, brick, smooth stucco and timber shall be required. Materials such as metal sheeting and excessive use of glass are inappropriate.

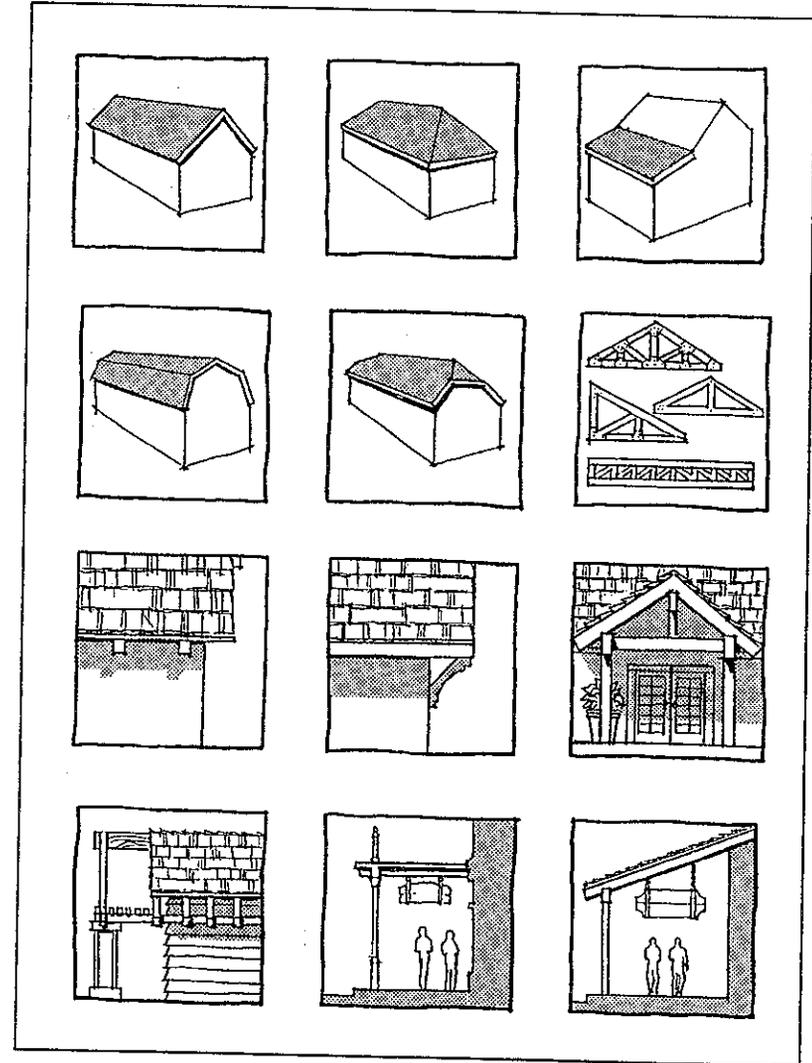
b. Roofing materials such as ceramic, concrete or terra cotta tiles; standing seam metal; pressure treated fire resistant wood shake; composition, or asphalt shingles shall be required.

c. Colors shall generally be earth tone, or otherwise subdued. Vivid colors as accents may be acceptable.

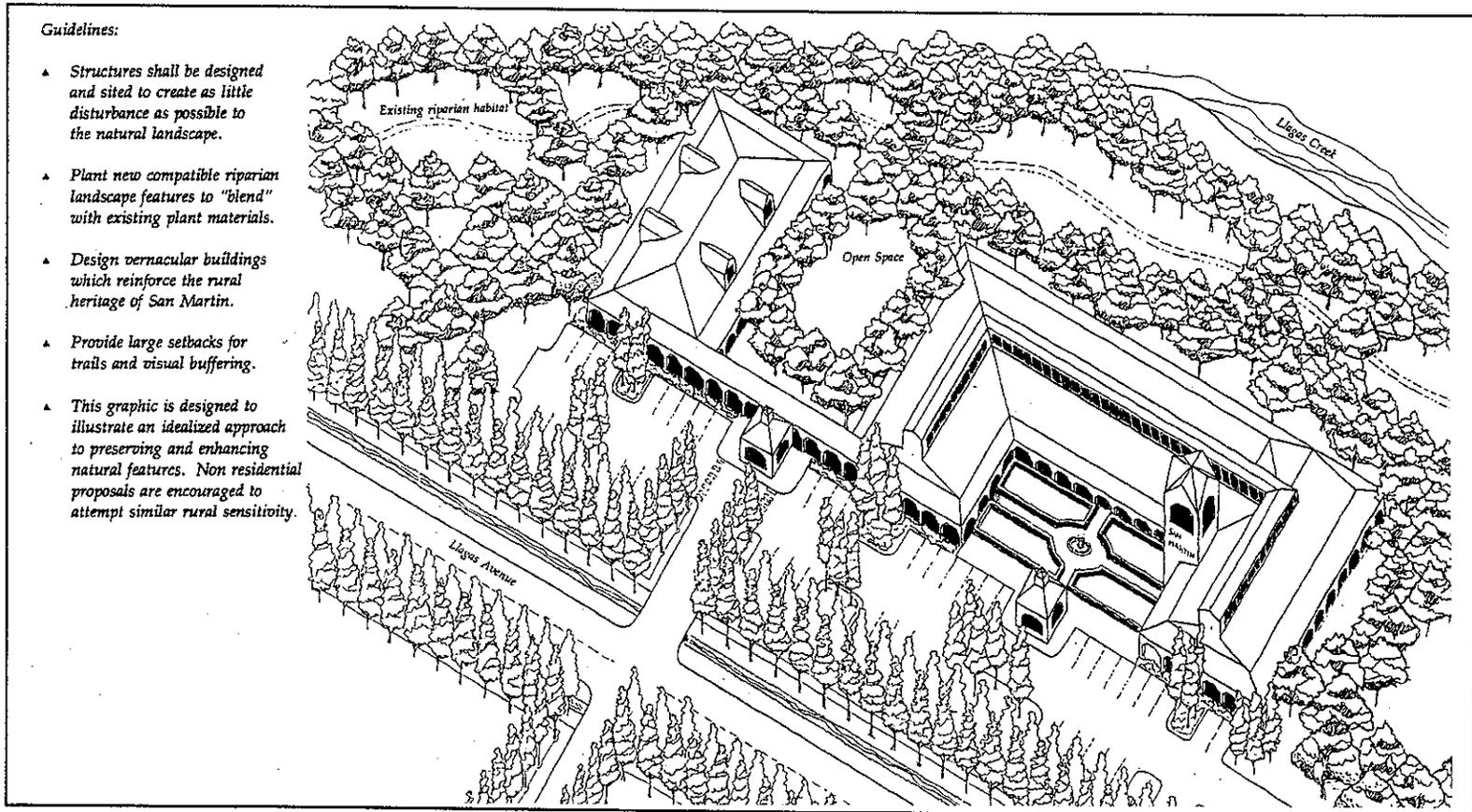


*Acceptable Wall Materials*

5. For larger structures or developments, a more complex building shape or a cluster of smaller buildings is appropriate rather than a single large monolithic building.
6. Building height shall be limited to two stories. A three story height limit may be allowed under the County's variance procedure if such a variation meets the mandatory variance findings, and adds to the desired visual character of the development.
7. Pitched roofs, generous overhangs, wide verandas and covered porches and walkways shall be encouraged while still meeting all other zoning and building code requirements. Flat roofs without western style parapets are inappropriate.



*Desirable Roof Forms and Elements*



*Ideal Rural Oriented Commercial Development That Fits In With Natural Landscape*

**D. Siting**

1. Structures shall be designed and sited to create as little disturbance as possible to the natural landscape.
2. Developments shall be designed and sited to protect creeks or creekside habitat. Existing established easements for pedestrian and equestrian trails should be protected.
3. Trail easements may be required as appropriate.

## **E. Landscaping**

1. A final landscape plan shall be reviewed and approved by the Architectural and Site Approval Committee and shall comply with the County's signage policies.
2. Preliminary landscape plans shall be submitted with application materials. Failure to submit such a plan shall render an application incomplete for further processing.
3. Complete final landscape plans shall be required as a condition of approval. Landscape plans shall include all existing and proposed landscaping or landscape features.
4. Landscaping shall consist of a combination of trees, shrubs and groundcover. Trees shall be a prominent feature of the landscape plan. Mature trees, as well as portions of orchards, shall be retained as part of the landscaping program whenever possible.

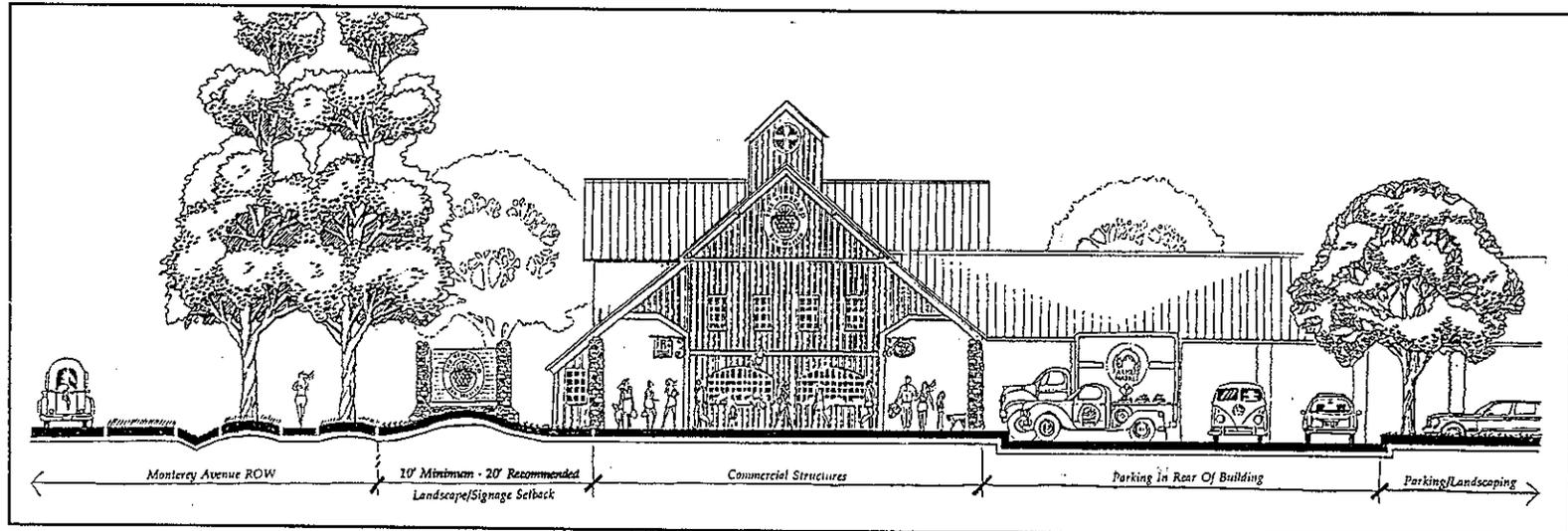
5. Landscaping shall relate to the entire development and shall be installed throughout the site and along street frontages.

a. Landscape plans for the "village core" of San Martin shall include the use of street trees, landscaped parking areas and landscaped "people places." Landscape strips along the front, side and rear sides of the property shall also be required as deemed appropriate by the ASA Committee.

b. Landscape plans for the properties along Monterey Road, south of the "village core," and along San Martin Avenue, between the Post Office and the San Martin Avenue/South Valley Freeway Interchange shall incorporate landscaping strips along the front, rear and side of the properties. While 20' park strips are recommended, landscape park strips should be no less than ten feet wide along the front, ten feet

wide along the sides and ten feet wide along the rear property lines.

Wider landscape strips and/or additional landscaping may be required to mitigate aesthetic impacts, or to enhance the visual character of the site.



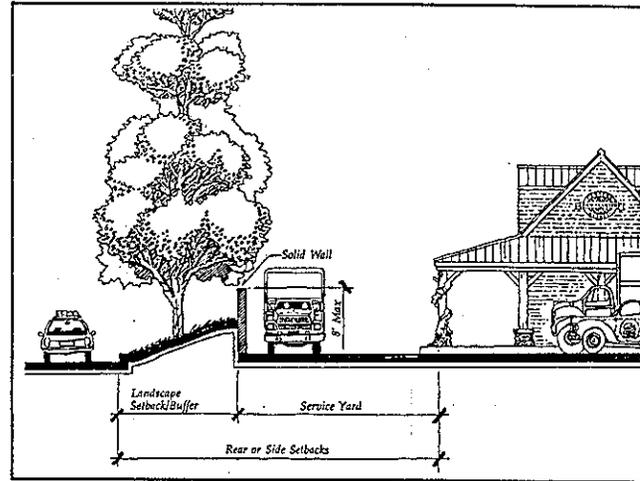
*Appropriate Monterey Avenue|Development Interface*

c. Landscape plans for the properties at the two interchanges and along the South Valley Freeway shall screen the use from view from the Freeway. Landscape plans for areas along the freeway shall enhance the scenic quality of the freeway by emphasizing tall

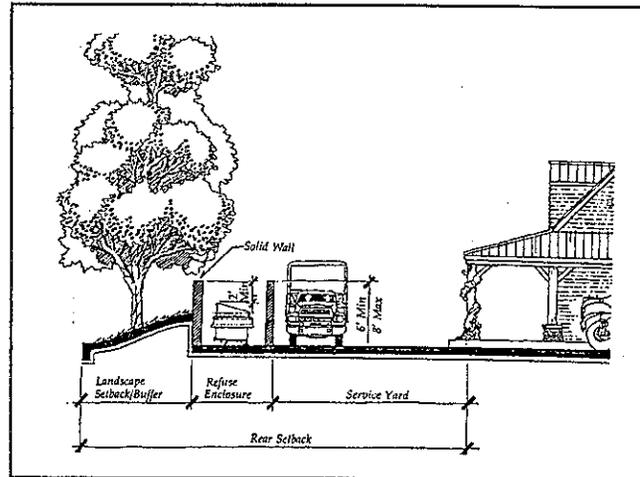
growing trees, white wood truss fencing, shrubs and other screening plant material. Plans shall include the use of landscape strips and extensive on-site, internal landscaping. Landscape strips should be no less than twenty feet wide along the front, ten feet wide along

the sides and twenty feet wide along the rear property lines. Wider landscape strips and/or additional landscaping shall be required to mitigate aesthetic impacts, to enhance the visual character of the site and to screen the structures and components of the use from view.

6. Applicant shall provide a plan for, and shall commit to the ongoing maintenance of all landscaping, including the installation and maintenance of irrigation systems.
7. Water requirements, soil conditions, ease of maintenance and intended function of plant materials shall be considered when selecting plant materials for the site.
8. The selection of plant material shall be based upon the projected physical conditions on the site and shall be reasonably resistant to drought and damage by insect pests or diseases. Native and naturalized plant species shall be utilized in the planting program.
9. Placement of plant material shall be designed to ensure that it will not interfere with traffic visibility or with the efficient functioning of a septic system.
10. When appropriate, service areas, such as water and fuel tank areas, loading docks and open storage yards shall be screened from view.



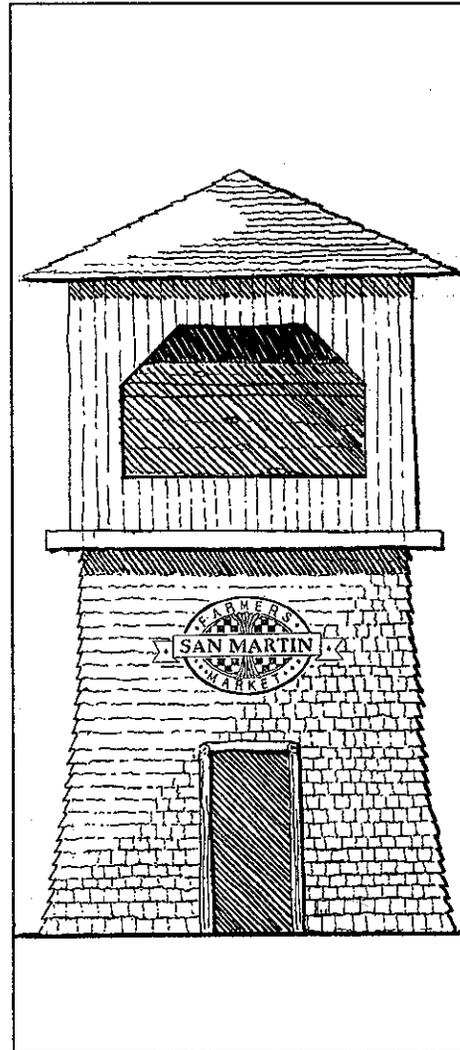
*Screening Option  
Loading Docks Service Areas*



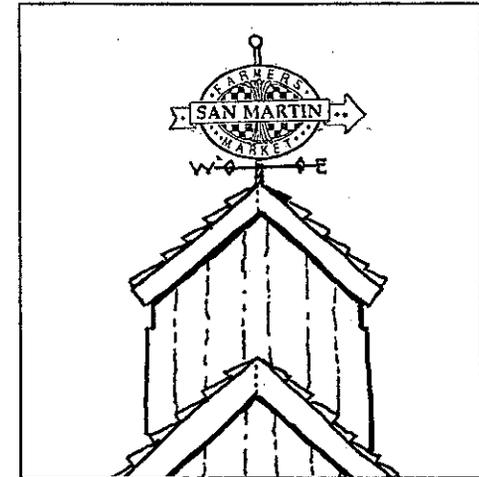
*Screening Option  
Trash Disposal*

**F. Signage And Lighting**

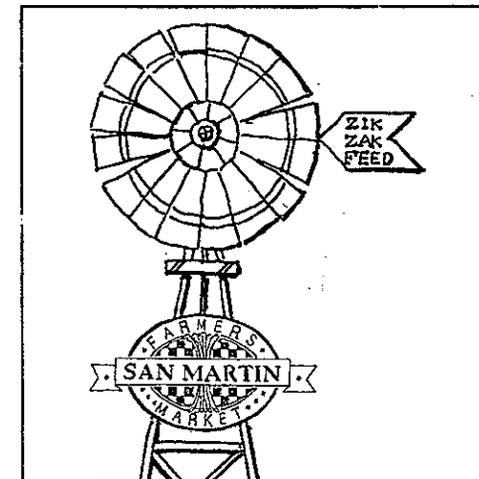
1. A signage program shall be reviewed and approved by the Architectural and Site Approval Committee and shall comply with the County's signage policies.
2. Signage on properties along Scenic Highway 101 (So. Valley Freeway) which are zoned with the -p (Scenic Preservation) combining district shall conform to Article 30 of the Santa Clara County Zoning Ordinance.
3. Use of rural icons such as wagons, farm implements, weather vanes, wine presses, hitching posts or antique water tanks and pumps shall be encouraged.
4. Lighting shall be low level, constant in intensity and color and shall be directed onto the property so as not to create glare and illumination on neighboring lands.



Water Tank

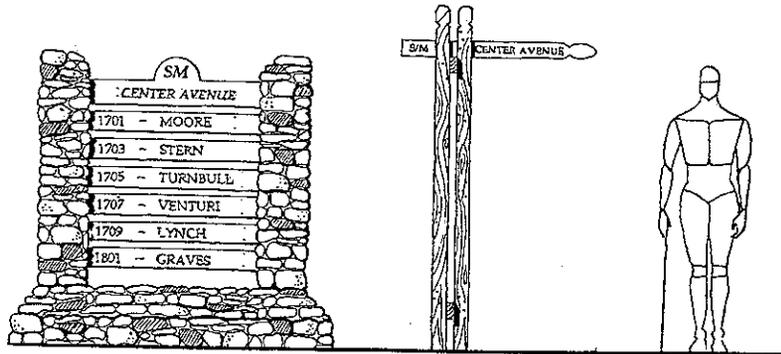


Weather Vane



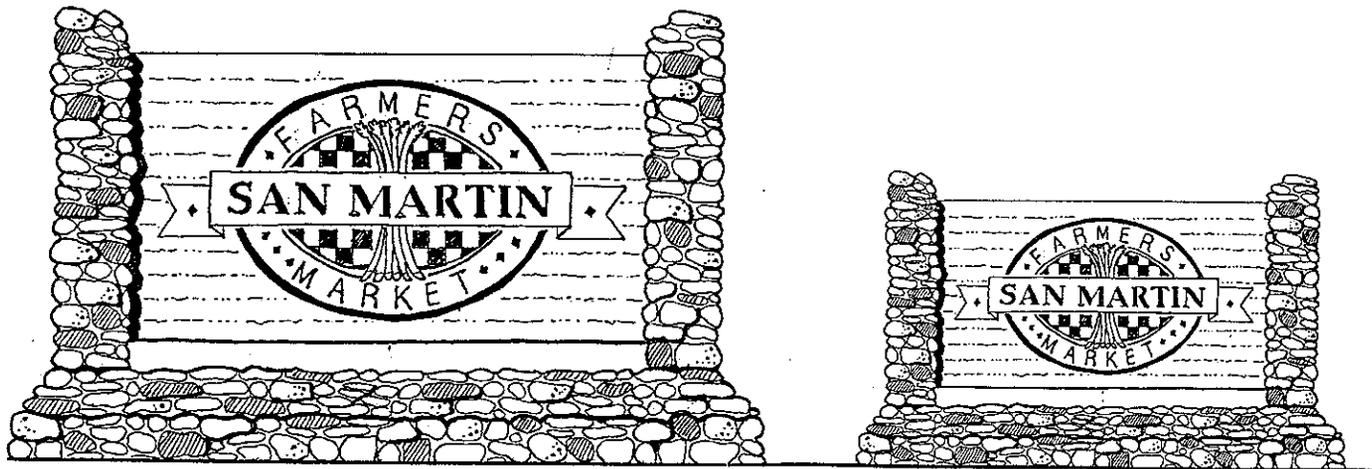
Windmill

*Signage Prototypes*



*Clustered Address Signage*

*Street Sign*

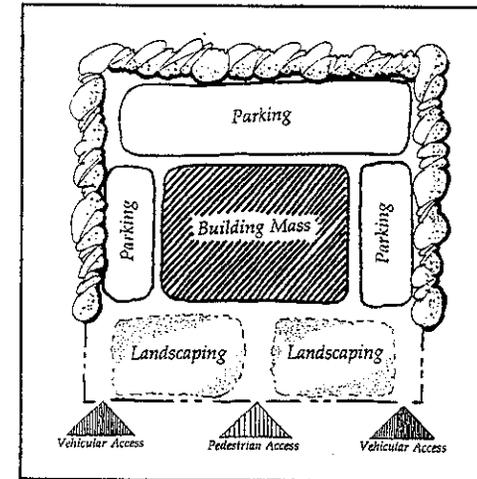


*Freeway Commercial Monument Sign*

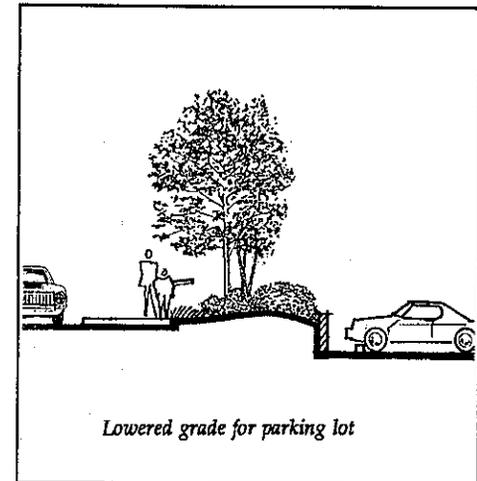
*District Commercial Monument Sign*

## G. Parking

1. Off-street parking areas shall be designed in accordance with the County's adopted "OFF-STREET PARKING STANDARDS" and Architectural and Site Approval Committee standards.
2. A parking plan shall be required at the time of application. Failure to submit a complete preliminary parking plan shall render the application incomplete.
  - a. For the "village core area" parking shall be located to the rear or side of the structures. Direct pedestrian access shall be maintained from the street to the entrance of the use.
  - b. For Monterey Road, south of the "village core" and San Martin Avenue, between the Post Office and the San Martin Avenue/South Valley Freeway Interchange, and properties at the interchanges, parking areas shall be designed to incorporate islands of landscaping to break up large blocks of parking spaces. Berms shall be used to screen parking and loading areas from view.
3. Adequate and separate areas for large vehicle parking (depending upon the use) shall be provided in the parking plan.
4. In commercial, industrial, institutional and professional office developments, off-street parking areas and driveways shall be treated with asphalt paving or better.



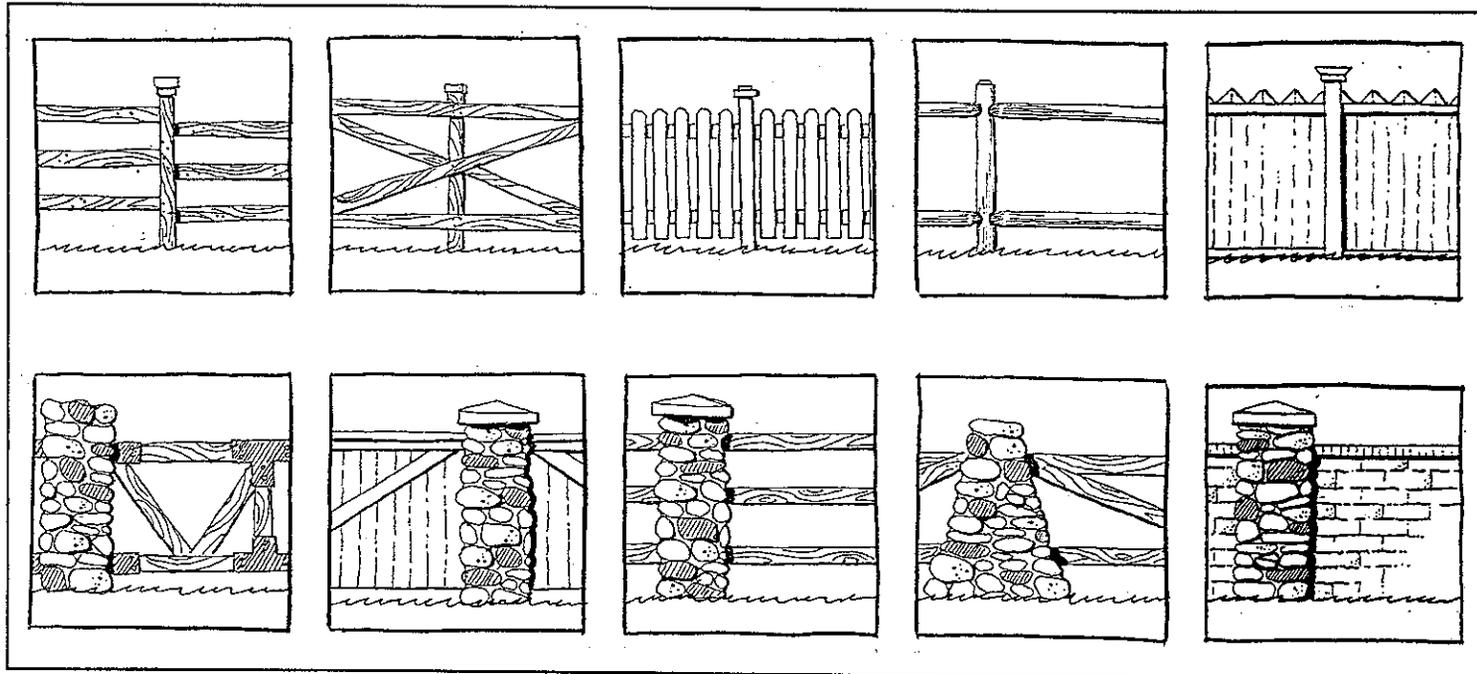
*Village Parking Concept*



*Screened Parked Cars*

## H. Fencing

1. Rural or natural looking fencing, such as wooden or split rail fencing, shall be used. Chain link fencing shall be discouraged (chain link with redwood slats or vines may be approved under very limited circumstances).
2. Open storage yards in industrial and commercial developments shall be screened by a combination of fencing and landscaping.





### III. FLOOD CONTROL AND DRAINAGE GUIDELINES

#### A. Llagas Creek And Tributaries

This section of the report sets out to identify a general drainage plan for the San Martin area as well as to provide illustrations of typical cross-sections for the different drainage systems in the area.

The Llagas Creek and its tributaries provide the basic drainage system for the San Martin area. The major flood control project in the area is the Llagas Creek Watershed project that is being carried out by the Santa Clara Valley Water District (SCVWD) with federal funding from the U.S. Soil Conservation Service and local funds from the SCVWD. This project has been designed and funded primarily to provide flood control protection to the more urbanized areas of Morgan Hill and Gilroy, which lie to the north and south of San Martin. Due to the low density rural

residential land use in San Martin, the section of Llagas Creek that flows through this area will be less improved. The project provides for this reach to be cleaned out and obstructions to the passage of flood waters, such as sand bars, removed. Therefore, some flooding of the area is likely to occur in the event of severe storm conditions.

In addition to Llagas Creek, there are also many other smaller creeks and waterways in the San Martin area. Historically, the agricultural plowing and leveling that has been done to develop productive fields has virtually obliterated or reduced the capacity of many of these small creeks and waterways that originally drained the area. The resulting seasonal flooding was not a problem to agricultural fields since it

usually occurred during fallow times and was probably beneficial to crop production.

However, when a landowner initiates a subdivision, drainage improvements must be made to protect potential residents from flooding. Since an area wide drainage system does not exist, the improvements are only made to the subdivided lot, resulting in a piecemeal approach to developing a drainage system. Although this is sometimes unavoidable in a rural area, it is not an adequate solution to area wide drainage problems. This section sets out to provide some very general guidelines for future development and reduce the previous parcel-by-parcel drainage approach.

In addition to the system of natural channels and waterways a certain degree of

flood protection is also provided to the area by its system of roadside swales. Since the majority of these swales were constructed privately, they do not follow any general design guidelines. In many instances they are not continuous and do not connect to a major waterway.

The problems of flooding that affect San Martin are a result not only of problems in Llagas Creek but also due to problems associated with local drainage. In Santa Clara County, local drainage is under the jurisdiction of the Cities and the County, while larger creeks are under the jurisdiction of the Santa Clara Valley Water District (SCVWD). When SCVWD modifies a creek, they design for a 100 year flood. Local drainage facilities are typically designed for a 3-10 year storm. The County of Santa Clara's Standards and Policies Manual, Volume 1 and SCVWD Ordinance 83-2 stipulate that property owners are responsible for the maintenance of natural water courses through their properties, except for those that have been accepted for maintenance by the County or the SCVWD. The District only maintains creeks where they have right-of-way. Not all rights-of-way are dedicated.

There are three levels of flood protection that are provided for in the San Martin area. First, Llagas Creek is the primary drainage facility that

provides some flood protection to the area. There is a detailed Flood Insurance Study on this creek and the National Flood Insurance Rate Maps show the areas that will be inundated in the event of a 100 year or one percent flood. If property is developed within an area subject to flooding from the one percent flood, the County's Standards and Policies Manual, Volume 1, stipulates some requirements that must be met. The manual states that County Ordinance Section C3-4 requires that the approving body provide assurance that the proposed development is substantially consistent with the need to minimize flood damage and that adequate drainage is provided for this purpose. It also requires that the developer furnish base flood elevation data.

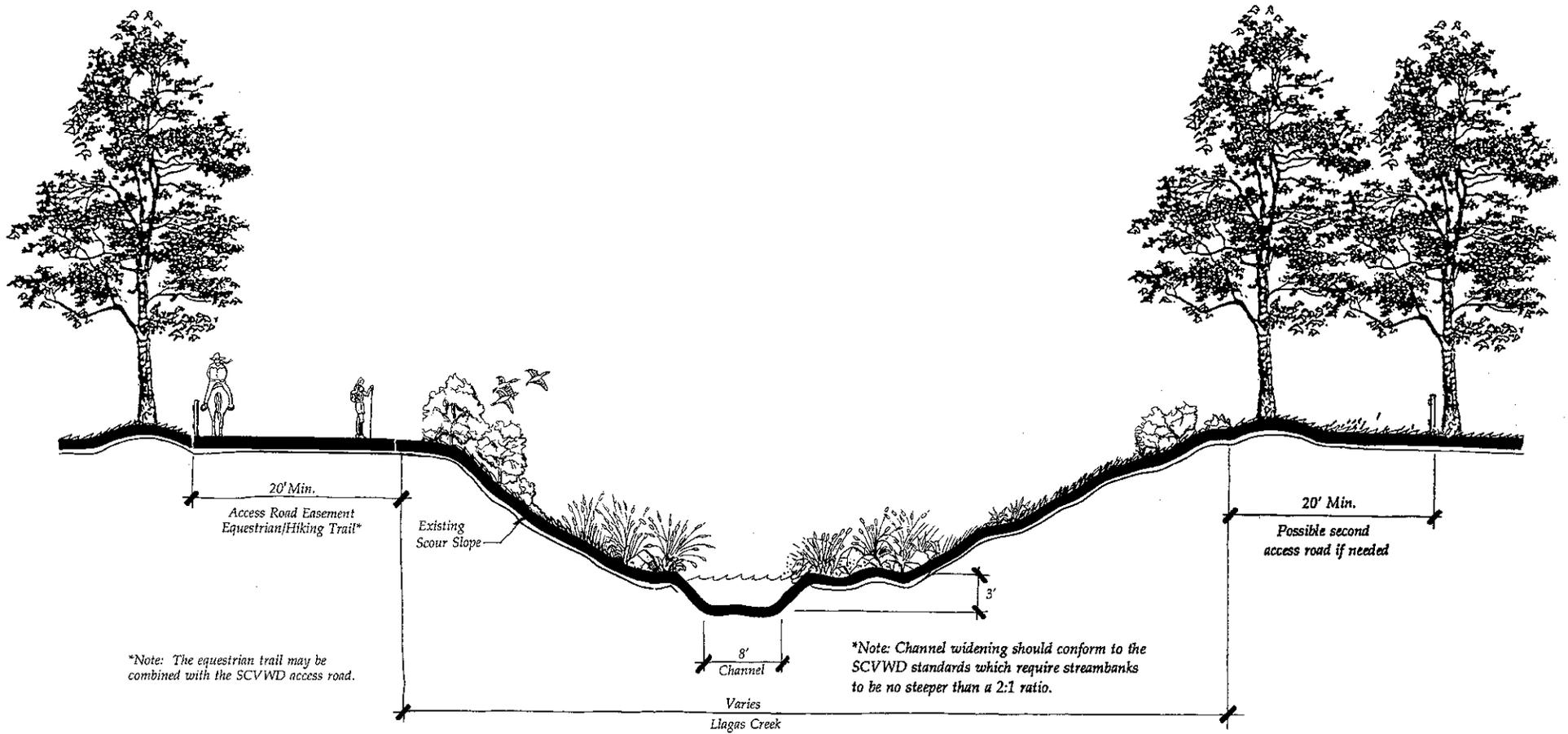
Cross-sections 1 and 2 are two alternative sections being proposed for Llagas Creek. It is proposed to use the creek and its adjacent areas as a dual purpose facility. It is recommended that a 20 foot wide access road surfaced with compacted decomposed granite follow the creek on the east side. This access road could also be used as a hiking/equestrian trail if agreements can be worked out with individual property owners to grant public access. It is also proposed that riparian trees be planted contiguous to the top edge of the creek bank

designed to: (a) enhance the natural habitat of Llagas Creek; (b) provide an aesthetic element; (c) provide shade for recreational activities during the hot summer months; and (d) function to moderate wind. Riparian plant material could be planted on the side slopes to enhance the natural habitat and add to the rural appearance of the area.\* A low flow channel is also shown in the channel bottom of both cross-sections.

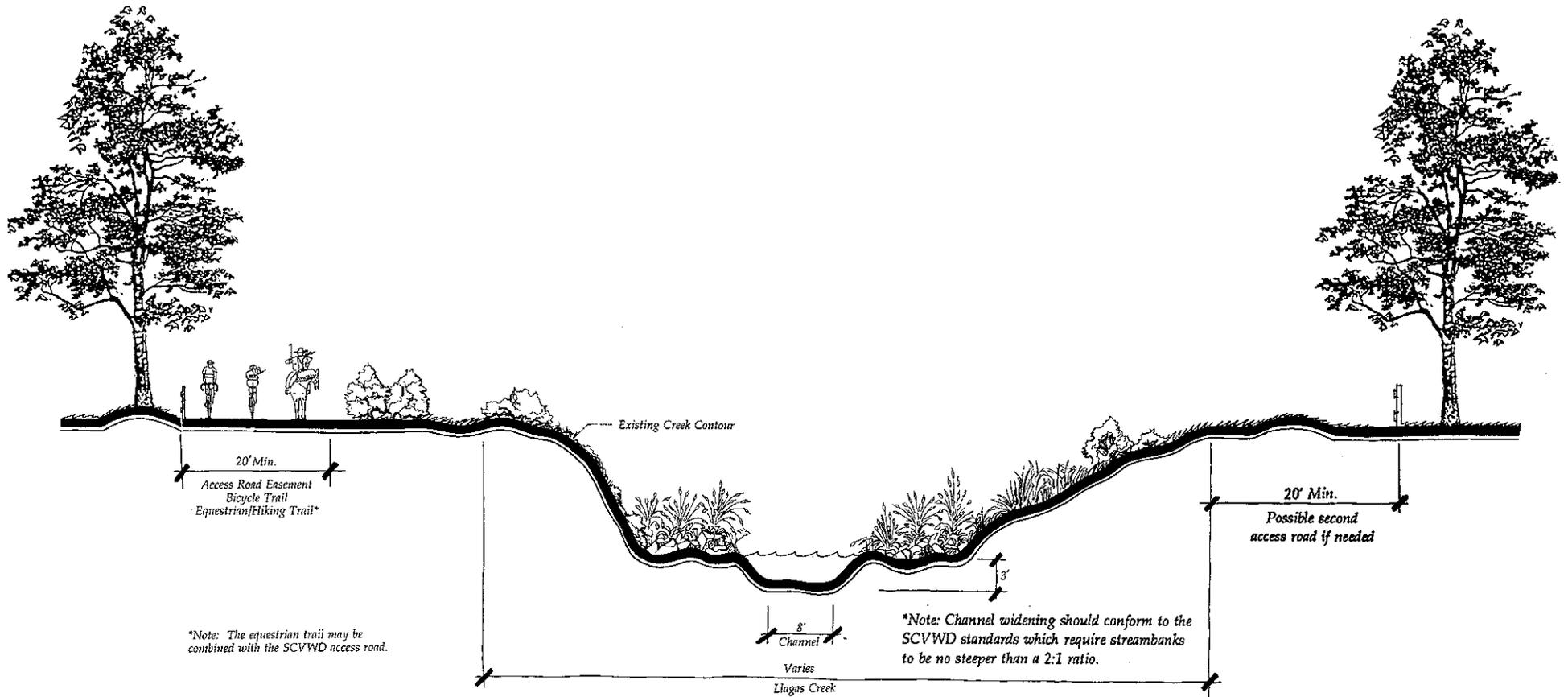
\* *Planting additional vegetation in the channel reduces its capacity to carry flood waters. Creating additional riparian habitat is desirable, but may necessitate widening of the channel to increase its ability to carry flood waters.*

The second level of flood protection provided to the area is from the smaller creeks and waterways. The SCVWD does not currently have any projects planned for these creeks. As mentioned previously, property owners are responsible for the maintenance of natural watercourses through their property except for those that have been accepted for maintenance by the County or the SCVWD.

# Cross-Section 1



## Cross-Section 2



Cross-section 3 is a typical section alternative proposed for the smaller creek areas. An access road/recreational trail is provided on one side of these drainage features that can be used for recreational purposes. These smaller creeks do not have the capacity to provide 100 year flood protection and hence development around the surrounding area should be limited to prevent major flood damage from taking place. It should be ensured that new development does not increase runoff to downstream areas, resulting in induced flooding of these areas.

The third level of flood protection is the local drainage provided by the roadside swales. As mentioned, these swales provide minimal 10 year flood protection and are under the jurisdiction of Santa Clara County. The County's Standards and Policies manual stipulates that draining to a roadside swale (County road or private road) is not considered an adequate drainage solution unless the ditch and culverts have the required flow capacity. The County manual also stipulates that any proposal for this type of drainage must be supported by a complete layout and analysis furnished by the applicant's engineer.

Since deep drainage swales could be traffic hazards and have many maintenance problems, they are generally not allowed in many areas of the County.

Cross-section 4 illustrates two alternative cross-sections that could be considered for roadside swales in San Martin. A triangular shape with a flatter slope on the side of the swale adjacent to the road is proposed together with a minimum eight foot wide shoulder that has a gentle 5% grade which provides space for vehicles.

A rectilinear swale characterized by shallow side slope and broad bottom is provided to accommodate temporary or occasional equestrian use.

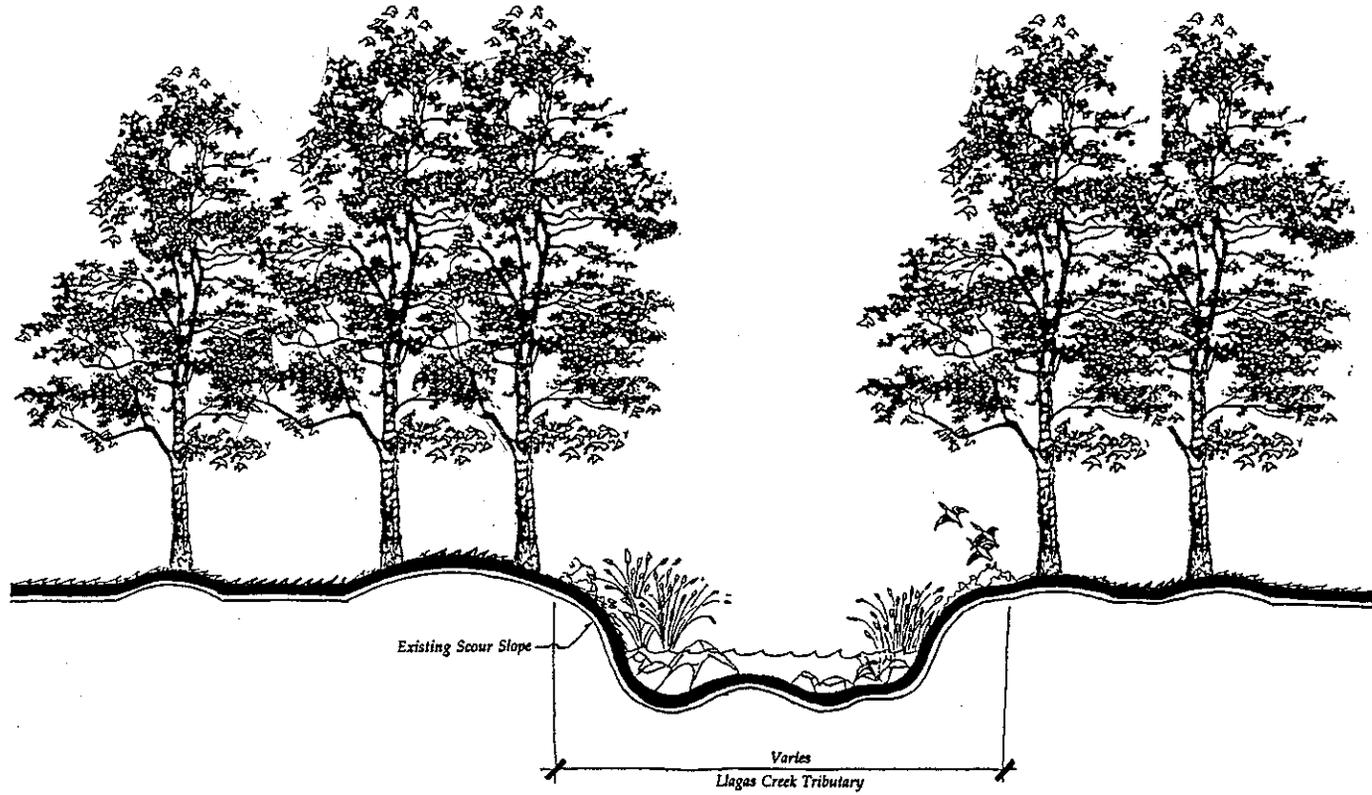
There are areas of San Martin that do not have direct access to the flood protection systems mentioned above. The County's Standards and Policies Manual states that development of these areas should not be authorized until it is demonstrated by the applicant's engineer that it can be adequately drained, that it will not cause problems to nearby property, that it is not subject to significant damage from the 1% flood, and that

the on-site drainage will be controlled in such a manner as to not increase the downstream peak flow or cause a hazard or public nuisance.

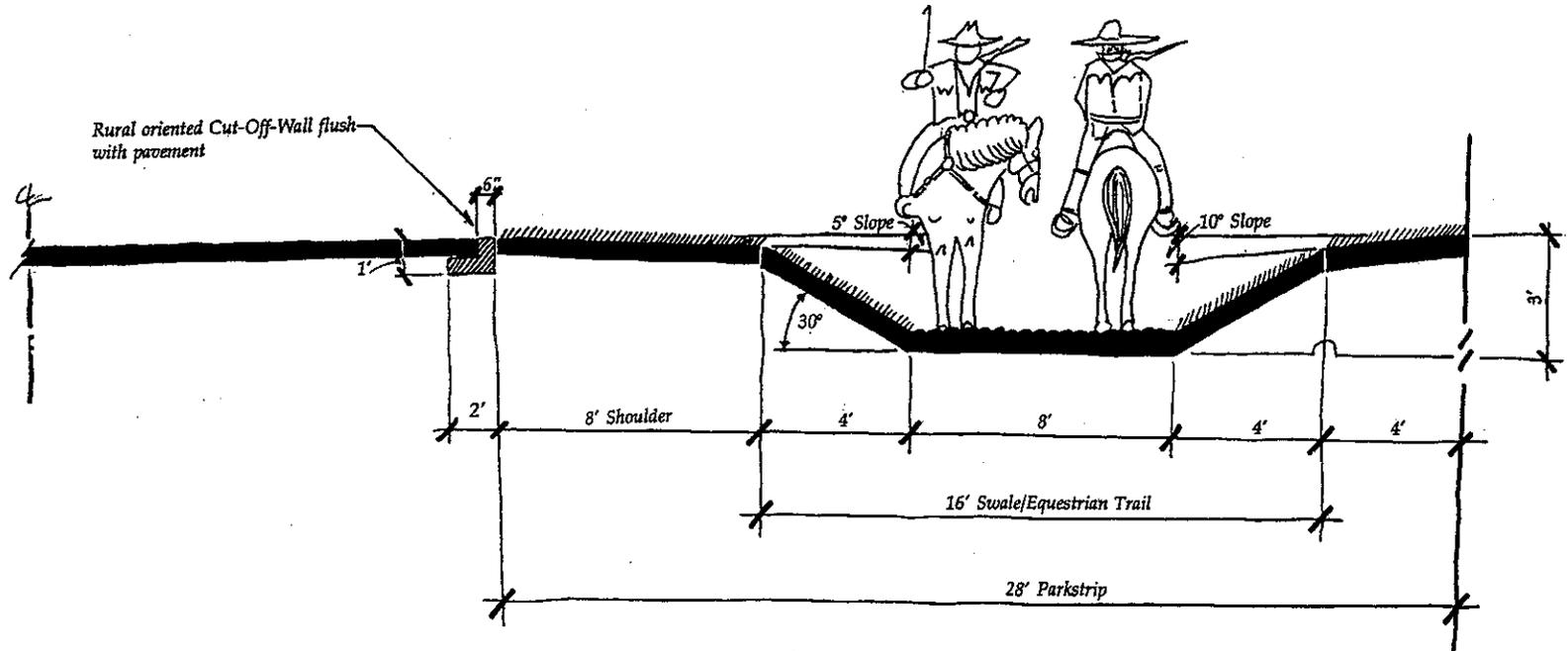
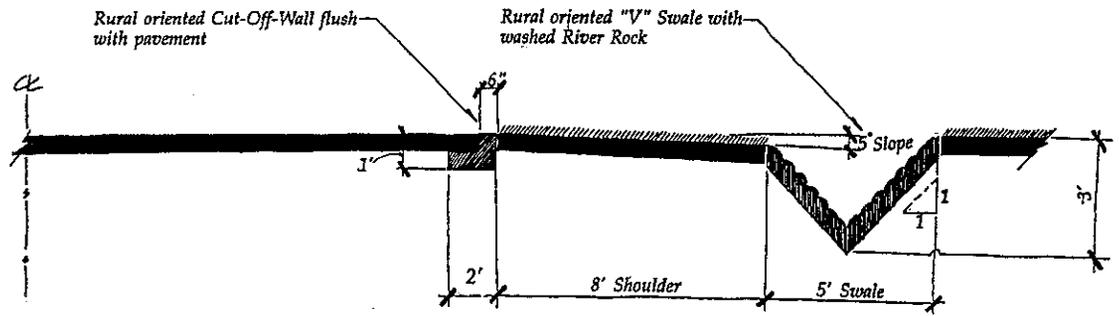
Retention or detention basins are one solution to this problem although they have inherent maintenance problems and in some instances may be pollution or safety hazards.

The County's Standards and Policies Manual states that, if allowed, they are considered to be temporary in nature and that fencing and other safety measures may be required. The County also requires adequate provisions for continued maintenance of these facilities and that these basins be designed for controlled, automatic and complete emptying in a manner that does not increase the downstream peak flow or any lesser flow rate that may be specified. If the emptying of the basin is to be by percolation, the County requires that the percolation rate be determined by field tests. The County also requires that the time period for emptying the basin after the design storm has passed, shall be 72 hours, or any other such period as specified or approved by the County Surveyor.

Cross-Section 3



# Cross-Section 4



## B. Creek/Tributary Preservation Guidelines

Llagas Creek and its tributaries should be protected through incorporation of the following:

- ▲ Do not allow culverting of the creek (except at road crossings and the like, per SCVWD standards).
  - ▲ Minimize significant constructed alterations of the creek or tributary channel.
  - ▲ Require adequate setbacks from the floodway to help maintain the riparian nature of the creek or tributary.
  - ▲ Clean the creek/tributary of trash and dead vegetation that could plug culverts.
  - ▲ Dedicate creek/tributary preservation and maintenance easements as part of the development of property along the creek or tributary.
- ▲ No development in the watershed should be sited on known environmentally sensitive areas (i.e. expansive soils, riparian vegetation, steep slopes, wetlands).
  - ▲ Physical design of sites and structures should include the following considerations:
    - *Buffer setback from top of creek bank (150' minimum recommended unless special circumstances require less)*
    - *Riparian/Windrow vegetation buffers along banks*
    - *Adequate sewage disposal*
    - *Preserve existing riparian vegetation*
- *Keep soil on the development site - protect cut and fill slopes with temporary diversions, berms, terraces, etc. to intercept or divert storm runoff*
  - *Do not strip natural riparian vegetation along Llagas Creek or its tributaries to prepare a development site*

It is recommended that Offers of Dedication along Llagas Creek and its tributaries be requested when the subdivision of creek or tributary front properties is proposed and where there is a reasonable expectation that a continuous riparian/recreation corridor can eventually be acquired.

### C. Retention Guidelines

Retention basins can be created in any natural depression, valley or low lying area. It is frequently necessary to construct a linear berm or dike to provide enough area for storage of water during the rainfall. These areas should be designed with devices to release the water slowly over a period of time following the rainfall. This release should be designed to duplicate the volume normally released from these areas prior to development.

It is also necessary to provide an emergency overflow for use during extreme rain periods to assure water is channeled away from urbanized areas. This method assumes that the heaviest rainfall will occur only once every four to five days, thereby allowing the release of water so storage for the next rain period can take place. Holding ponds, if carefully designed, could also serve multiple recreation use during the summer months, although such use has not occurred in the County to date. These include soccer fields, youth football, and little league Baseball.

Drainage retention basins should incorporate design features to lessen their visual impact such as:

- ▲ Design basin depth to three feet or less, unless otherwise required/ approved so that fencing is unnecessary.
- ▲ Design basin shape to appear natural - avoid rectilinear forms.
- ▲ Provide meandering slopes and banks.

#### **D. Parking Lot Drainage Guidelines**

Parking lots and other large level areas (e.g. playfields) can also be used as holding basins. Generally the temporary loss of parking spaces during storms presents few problems. This technique would drain the water from the bulk of the parking lot towards one corner, where it would be contained by berms. By careful design, the loss of parking spaces during storms can be minimized, while accommodating the maximum amount of runoff.

Generally, a low area away from the buildings is selected and graded with dams and berms to retain water. The area would have a drainage system designed to allow a portion of the water to percolate into selected landscaped areas or through small drain openings. Runoff that will be released into the storm drain system may require some form of filtration to remove oil and grease. Most of the water can be stored until the rainfall ceases. It can then be disposed of through groundwater percolation, evaporation, or dispersal over grassy areas over a period of several days.

**E. Southwest San Martin  
Area Interim Development  
Guidelines And Policies  
Of The South County  
Joint Area Plan**

The following development guidelines focus and supplement adopted General Plan policies as they are to be applied to the southwest San Martin area bounded by Highland Avenue to the North, Fitzgerald Avenue to the south, Santa Teresa Expressway to the west and Monterey Highway to the east. These guidelines, along with General Plan policies are to be used in reviewing and conditioning development proposals.

- ▲ Measures for flood control and local drainage should be consistent with the policies of the South County Joint Area Plan. (See following policies.)
- ▲ In addressing flood control and drainage for the area, the stream environment, natural riparian areas and floodways should be restored, and/or protected and enhanced.
- ▲ Projects should be designed to provide appropriate on-site detention, when needed, and drainage to the creek or other acceptable facility. Provision should be made to assure mitigation of runoff impacts down-stream. A grading plan should demonstrate how the project will achieve these objectives.
- ▲ In dealing with surface runoff, alternative approaches to drainage ditches should be considered where appropriate such as drainage swales and/or retention ponds. In appropriate situations, these should relate to open spaces in the subdivisions.
- ▲ Roadside ditches should be used only for drainage of the road, not for property drainage.

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**SOUTH COUNTY JOINT AREA PLAN: Flood Control Policies**

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- SC 12.0 Since flooding affects substantial areas of South County, and the flood control projects now being constructed are designed to protect only existing developed and planned urban areas, land development should be managed by the three jurisdictions to mitigate flooding problems and minimize the need for local public funding for additional flood control and local drainage facilities. Flood damage in South County should be minimized through a combination of actions. In flood-prone areas, inappropriate development should be prevented through land use planning, urban development policies and land use regulations. Areas which are developed or planned for development should be protected by the construction of flood control facilities. Development should be managed through advanced planning and design standards to minimize off-site flooding and drainage problems.
- SC 12.1 Highest priority for construction of flood protection facilities should be given:
- a. First, to areas of existing development subject to the highest potential flood damage;
  - b. Then, to undeveloped areas planned for urban development which would be subject to the highest potential of flood damage;
  - c. Then, to agricultural lands; and
  - d. Finally, to other undeveloped areas.
- SC 12.2 If federal and state funds are not available for future flood control facilities and such facilities must be funded locally, those property owners who would benefit from and those who would contribute to the need for such facilities should pay the cost.
- SC 12.3 Developers whose proposed projects would induce downstream flooding should be required to provide mitigation to eliminate the flood-inducing impacts of their projects.
- SC 12.4 Streamside development should be designed in such a way as to facilitate maintenance of the waterway and protection of the environment and riparian areas. Careful consideration should be given to the use of streets to separate urban streamside development from the waterway consistent with Santa Clara Valley Water District recommended streamside street designs.
- SC 12.5 If development is to be allowed in flood-prone areas, flood control facilities or appropriate flood-proofing should be provided prior to or in conjunction with development at developers' expense.
- SC 12.6 Where other mitigations do not solve the flooding problem, raising individual foundations (padding up structures) may be a solution; however, its use must be restricted in order to minimize the cumulative effects on adjacent areas.
- SC 12.7 The Cities and the County should require mitigation of any stormwater runoff produced by development that occurs beyond that described in the 1981 General Plans of the County and the Cities as of 1982.

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**SOUTH COUNTY JOINT AREA PLAN: Flood Control Policies (Continued)**

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- SC 12.8 All local development should provide appropriate mitigations of off-site impacts. These may include: limiting runoff to pre-development levels and/or complete solutions to flooding and local drainage problems in the vicinity of the development. Methods may include: detention (storing runoff temporarily and then releasing it) or retention (storing runoff on-site for percolation).
- SC 12.9 Careful consideration should be given to the cumulative effects of development which would drain into the upper reaches of Llagas Creek and other creeks in order to avoid the need for channelization and consequent destruction of its riparian vegetation and natural habitat.
- SC 13.0 Local drainage problems in South County should be minimized by preventing inappropriate development in areas which are prone to drainage problems and by using design standards and advanced planning to manage development. Developers of individual projects should be required to mitigate off-site and on-site impacts and, where appropriate, to install local drainage facilities which would contribute to an eventual areawide solution to the local drainage problems, preferably in the context of a master plan for local drainage which should be developed jointly by the Cities and the County.
- SC 13.1 Since County maintenance is limited to maintaining local storm drainage facilities which may affect County roads, any additional storm drain-related maintenance beyond that which is currently provided will require additional funding from residents and/or developers.
- SC 13.2 Those residents who benefit from as well as those who contribute to the need for local drainage facilities should pay for them.
- SC 13.3 The County and Cities should require a storm water management plan for each development. This plan, which would be presented early in the development stage, would describe the design implementation and maintenance of the local drainage facilities.
- SC 13.4 The Cities and County should coordinate in the development of a master plan for local drainage. The master plan should include consideration of the interface between unincorporated areas and the City drainage systems.
- SC 13.5 Each development should provide mitigations of off-site and on-site impacts, as appropriate. These mitigations may include limiting runoff to pre-development levels and/or complete solutions to local drainage problems in the vicinity of the development or downstream. Methods may include detention or retention, with appropriate protection of groundwater quality.
- SC 13.6 Development should be designed to conserve soil and avoid erosion.

## IV. CIRCULATION GUIDELINES

### A. Introduction

Streets and roadways are the most significant man made element occurring in the San Martin community. As such, they can either support or negate the rural lifestyle. The guidelines offer several methods of "softening" or "ruralizing" the visual appearance of country roads and roadway edges through the provision of landscape features, alternative right-of-way widths, pedestrian trails and equestrian bridle paths designed to evoke a pastoral image.

During the Community Visioning Process, many San Martin residents expressed the opinion that adopted roadway standards and ultimate right-of-way standards currently used by the County require considerably more concrete and asphalt than they like or feel is necessary and, thus, contribute to the loss of rural ambiance in the community.

At present, the County has a variety of standards for the roadways under its jurisdiction. The San Martin Community is desirous of establishing alternative County road standards tailored specifically to the rural environment and lower traffic volumes of rural San Martin.

Ultimate planned rights of way on major roads in San Martin exceed 90 feet. Residents expressed a desire to have the County reconsider these adopted widths due to:

- ▲ The urban nature of such wide widths.
- ▲ The time involved to realize these full ROW improvements.
- ▲ The requirement for extensive land purchase by the County to acquire the ultimate ROW.

San Martin residents also expressed concern regarding the safety of pedestrians,

bicyclists and equestrians who currently utilize unimproved, substandard County roads without the benefit of separated, designated bike lanes or equestrian/pedestrian trails.

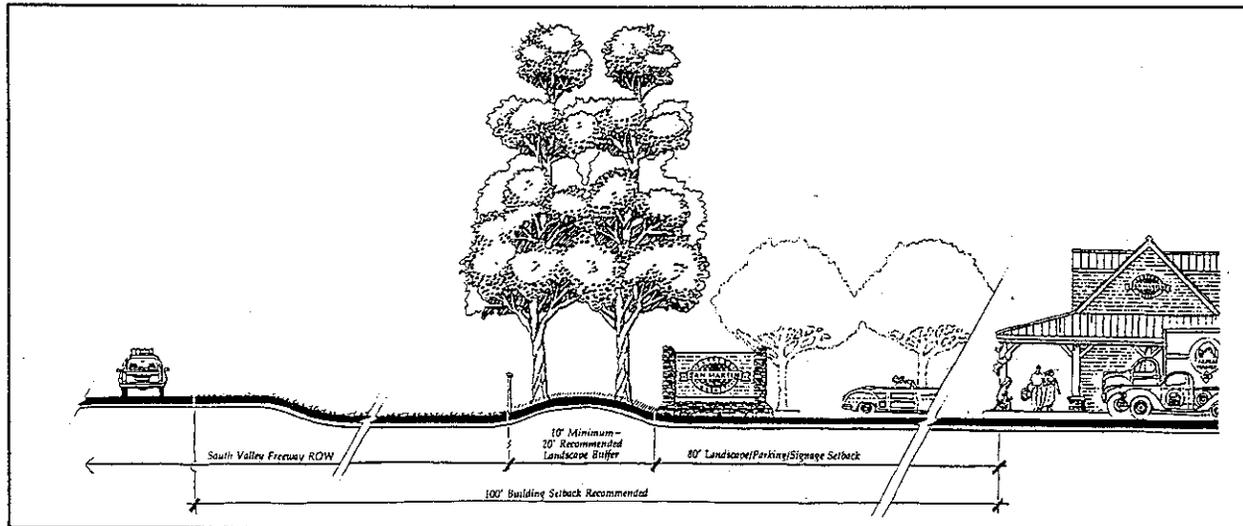
Potential solutions to this safety issue include providing improved separated pedestrian/equestrian and bicycle circulation features, more four way stop signage at selected intersections, lighting, and white reflective painted pedestrian crosswalks at selected intersections. These features used in concert could go far in discouraging motorists from speeding along San Martin's country roads and using such roads as Center Avenue as a speedy alternative to Highway 101.

The following guidelines illustrate several possibilities for minimizing the urban road sections utilized by the County and creating a more rural ambiance.

## B. Roadway Types And Alternative Cross Sections

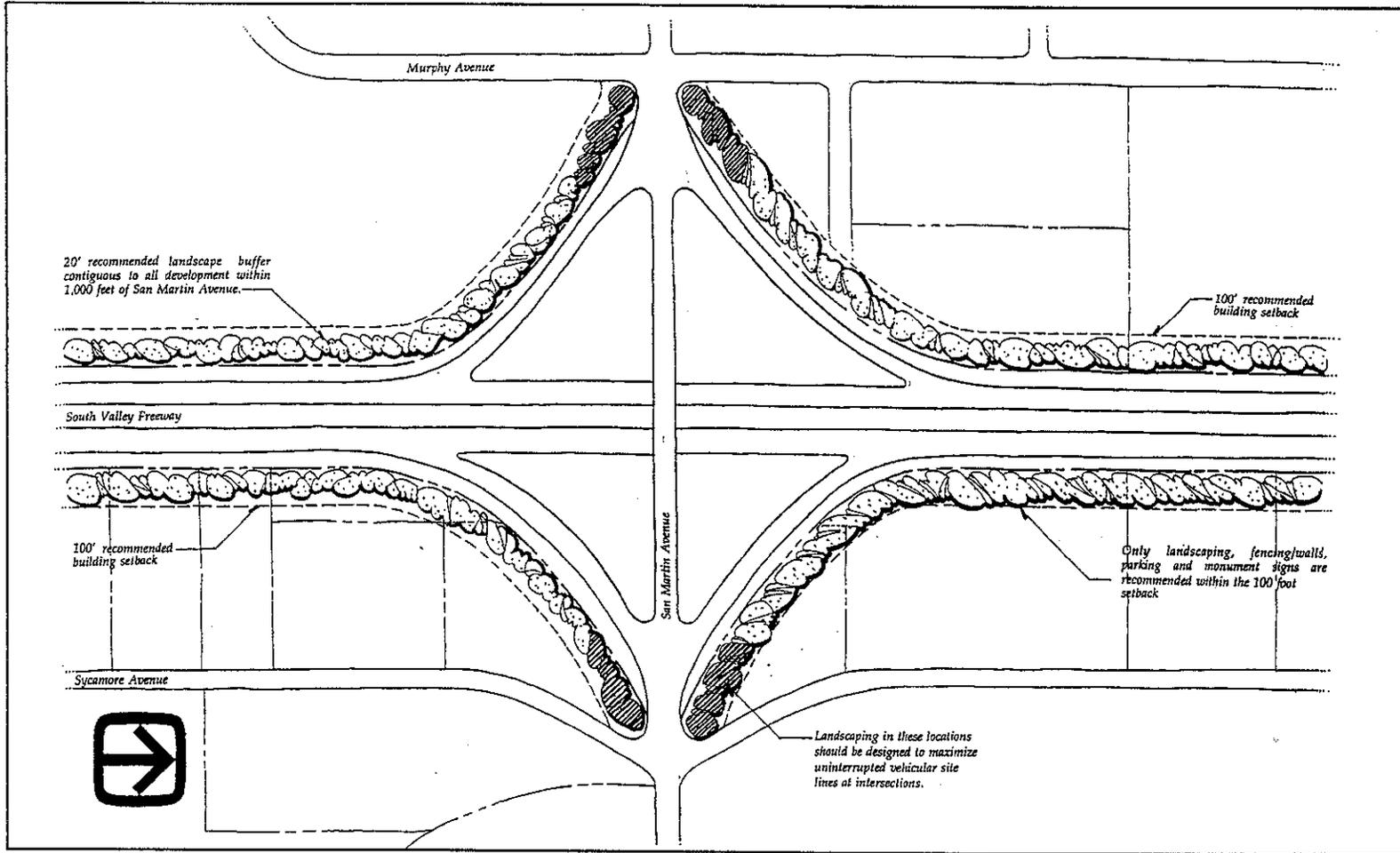
### 1. Scenic Roadway Corridors

Scenic roadway corridors best describes the main travel corridors through the San Martin community. Currently, Highway 101 (South Valley Freeway) is the only officially designated scenic corridor in the area. Special development interface and landscape buffering guidelines are illustrated on the following pages for development which is adjacent to this important scenic corridor.



*Development Interface with Freeway Scenic Road Corridor*

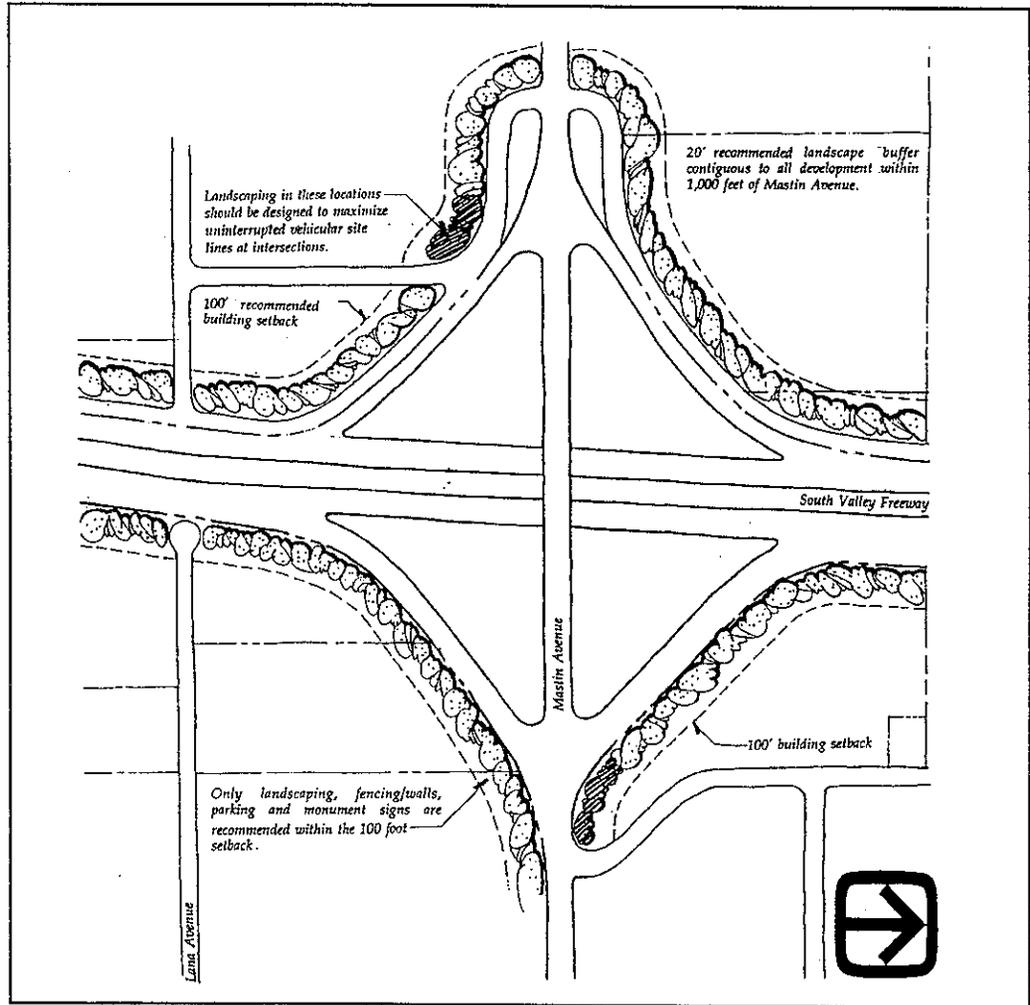
Freeway On/Off Ramps



▲ Setbacks are from roadway surface

Freeway at San Martin Avenue

Freeway On/Off Ramps



▲ Setbacks are from roadway surface

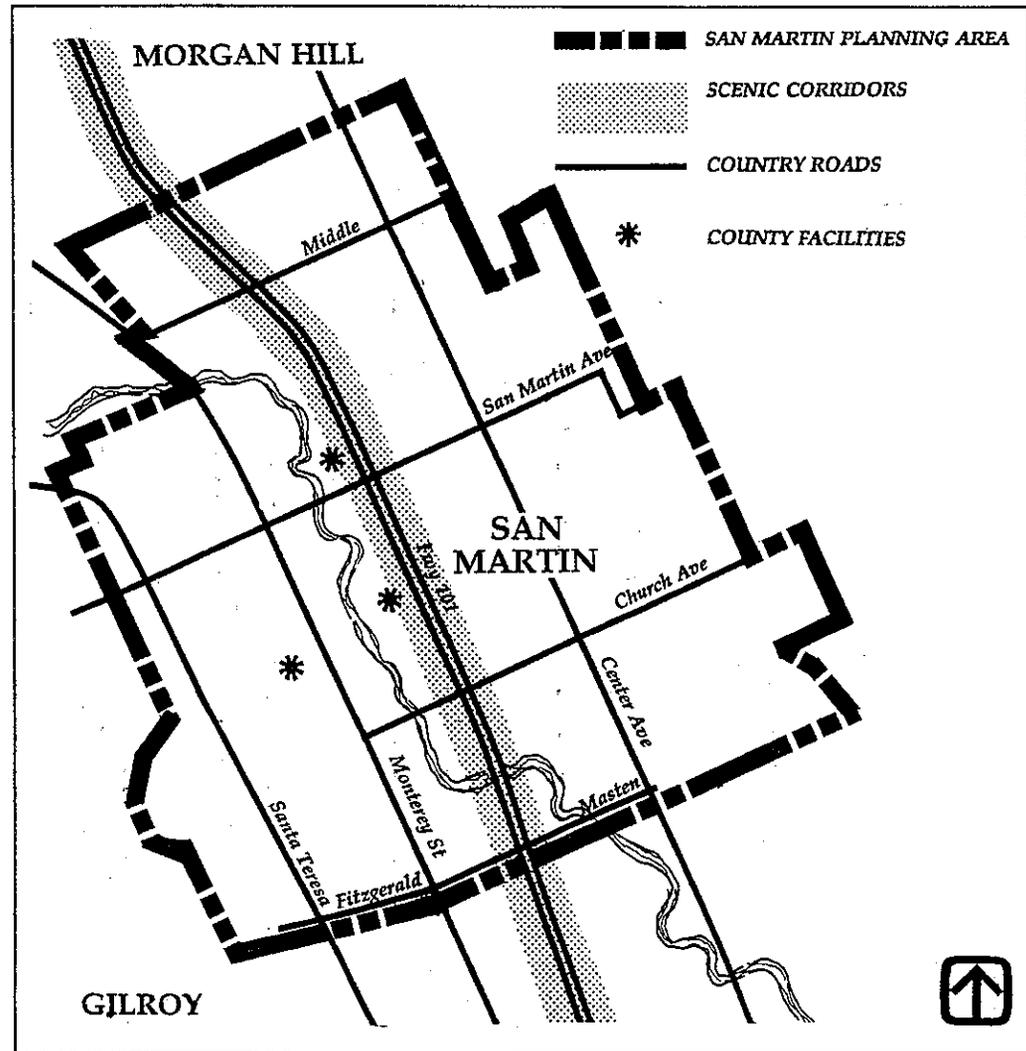
*Freeway at Mastin Avenue*

## 2. Country Roads

As currently structured, the County does not have an alternative reduced standard for "country" roads. The residents of San Martin believe that a critical design component in maintaining a rural atmosphere for their community is to create a special County designation for roadways which might be developed to a less "urban" standard than is currently required by County standards for these selected roadways. Residents expressed the opinion that the loss of rural ambiance along various roadways within San Martin would impair their ability to maintain a rural atmosphere.

The following roads are proposed as "*Country Roads*" in the community of San Martin. These proposed Country Roads are those roadways/arterials which have ultimate rights of way of approximately 92' to 110'.

- ▲ San Martin Avenue (east-west; 110' future width line).



- ▲ Masten Avenue (east-west; 110' future width line).
- ▲ Santa Teresa Boulevard (north-south; 110' future width line).
- ▲ Church Avenue (east-west; 92' future width line).
- ▲ Middle Avenue (east-west; 92' future width line).
- ▲ Center Avenue (north-south; 65' future width line).

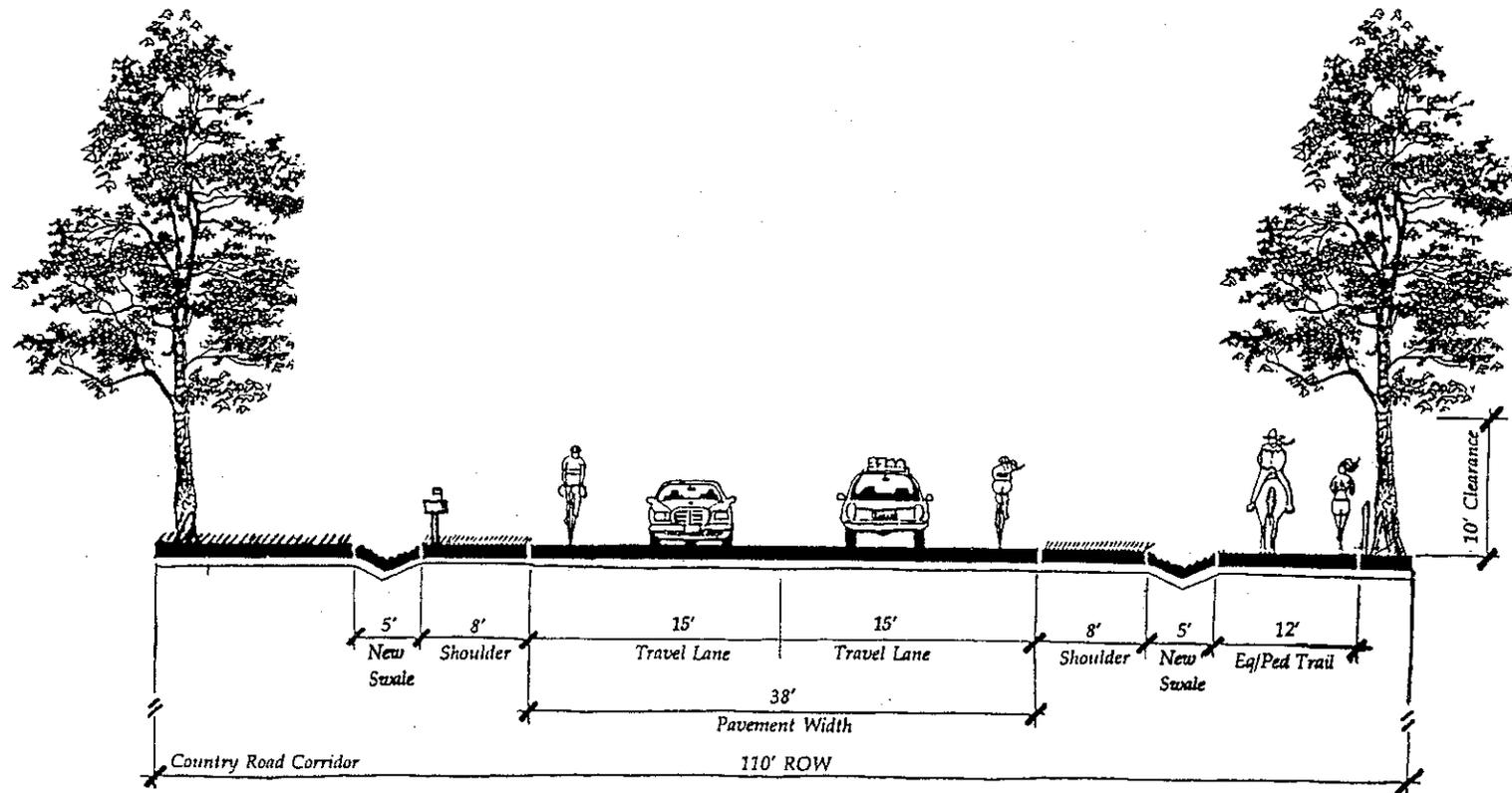
Special trail linkages and rural landscape features will characterize these circulation features. New character enhancing and safety improvements will be implemented throughout the San Martin community, as a design requirement or condition of approval in relation to the construction of new public roads, new development and for reconstruction of existing roads.

In the future these roads may include enhancements such as roadside trails, equestrian crossings, bicycle lanes and improved drainage swales.

Prototypical examples are included on the preceding pages. These examples are provided to depict the variety of right-of-way situations; however, it is realized that certain circumstances may limit these "desirable" or "alternative" designs to accommodate various functional needs.

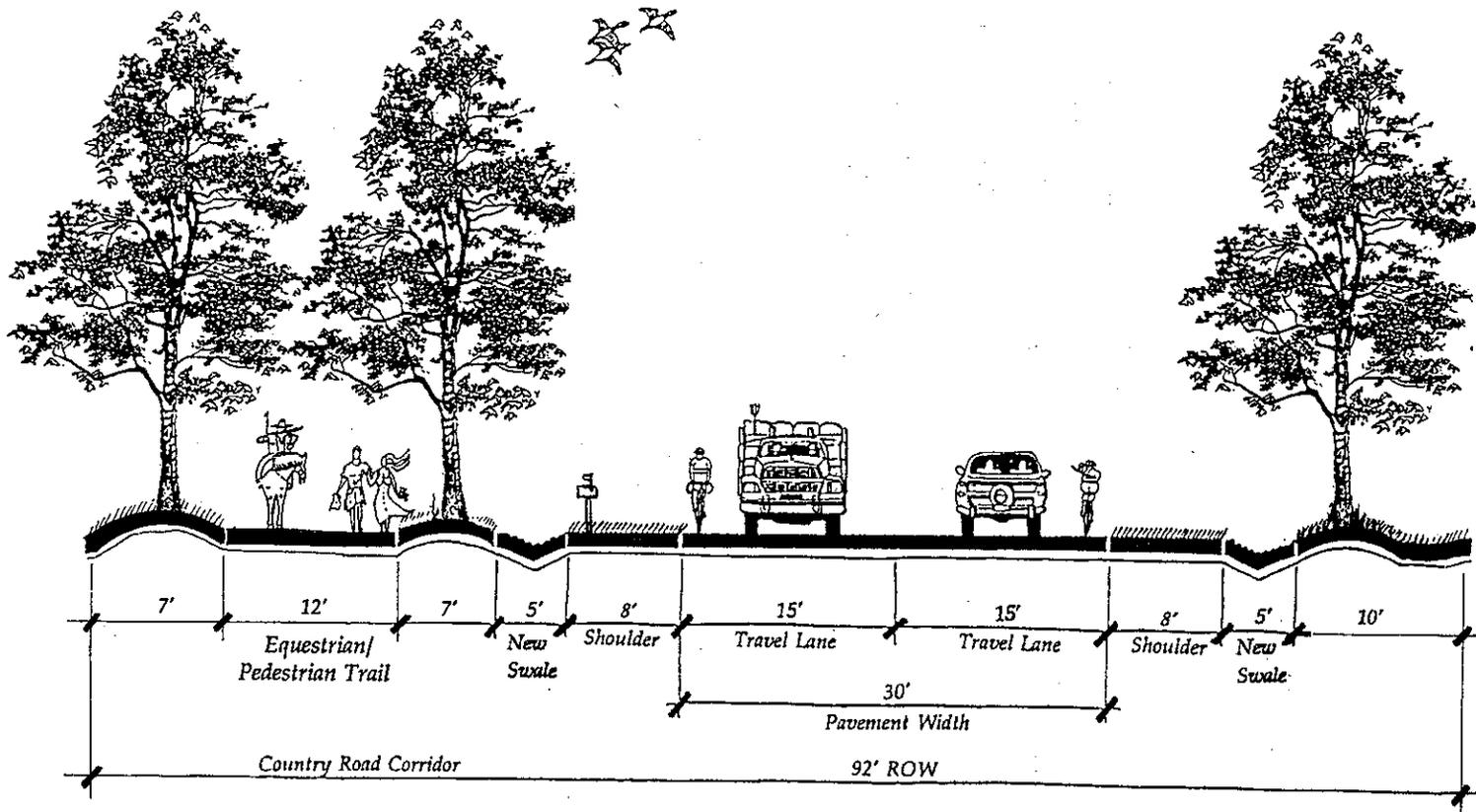
Country Road Alternative #1  
 110' Ultimate Right-of-Way - Two Lanes

- ▲ Pavement wide enough to accommodate bike travel in both directions
- ▲ 8' shoulder pull off area
- ▲ Shared equestrian/pedestrian trail 13' from travel lane
- ▲ Street trees
- ▲ 15' travel lanes



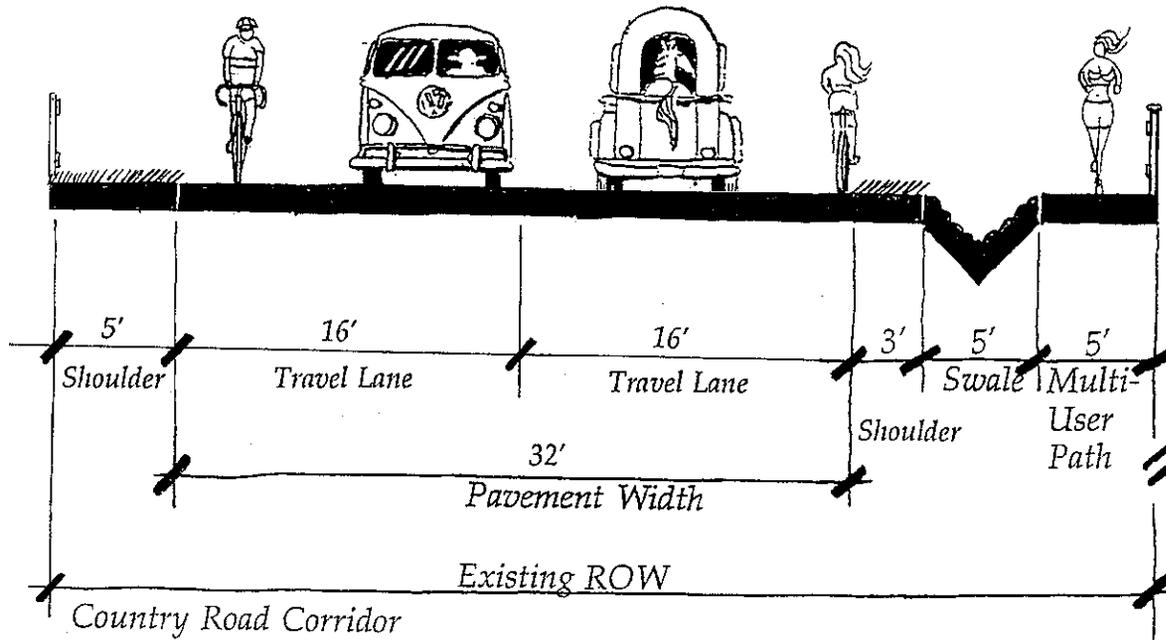
Country Road Alternative #2  
 92' Ultimate Right-of-Way - Two Lanes

- ▲ Pavement wide enough to accommodate bike travel in both directions
- ▲ 8' shoulder pull off area
- ▲ Shared equestrian/pedestrian trail 20' from travel lane
- ▲ Street trees
- ▲ 15' travel lanes



**Country Road Alternative #3**  
**Within Existing Right-of-Way - Two Lanes**

- ▲ Pavement wide enough to accommodate bike travel in both directions
- ▲ Narrow shoulders
- ▲ Multi-user path on one side
- ▲ No landscape
- ▲ 16' travel lanes
- ▲ Drainage swale on one side



## C. Safety Related Improvements

The residents of San Martin are very concerned with the safety of motorists, pedestrians, bicyclists and equestrians along their local roadway network.

Problems that are frequently cited are:

- ▲ Vehicles "running" stop signs.
- ▲ Excessive vehicular speed.
- ▲ Traffic utilizing local roads as an alternative to the freeway.
- ▲ Trucks utilizing local roads to avoid freeway weigh stations.
- ▲ Pedestrians, bicyclists and equestrians utilizing a narrow shoulder for local circulation.

The residents of San Martin face the dilemma of how to make their roadways safer without utilizing "urban" traffic control devices such as traffic signals, lower speed limits and more frequent stop signs. Other devices include special white reflective striping, warning signs, painted crosswalks and more law enforcement.

This plan cannot effectively recommend "rural" devices other than the implementation of the Country Road Standard which effectively separates cars from pedestrians. This plan cannot induce law enforcement agencies to increase local patrols. The signs on the following page could be posted more frequently to warn motorists of likely hazards. Beyond these recommendations the local community must reconsider the use of "urban" traffic devices that received the most local support. These included:

- ▲ Locate more four way stop intersection locations in San Martin.
- ▲ Request that the County post reduced speed limits on roads that are currently "unposted."
- ▲ Request that the County post additional crossing signs on approach to intersections.

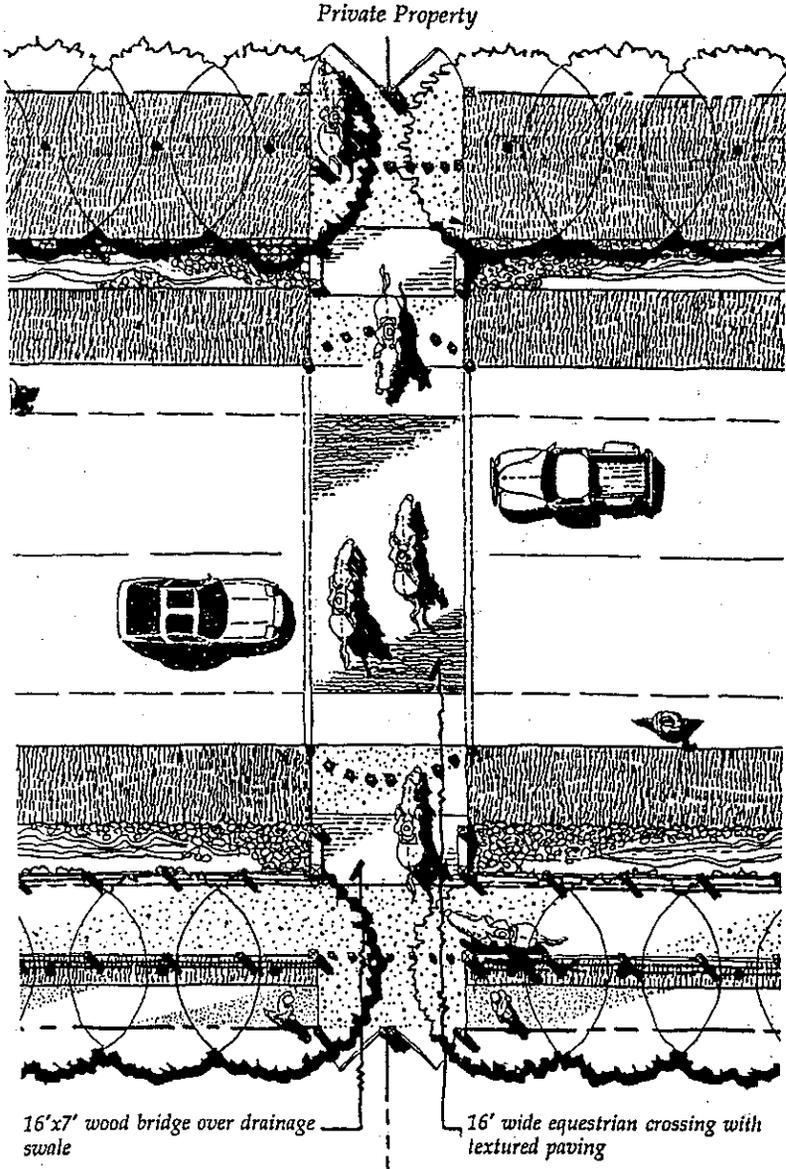
D. Roadway/Trail Material Specifics

TABLE 1

Development Type: Roadway and Trail Surfaces

Type	Recommended Surfaces
Public Road	Asphalt or Concrete, per County Standards
Private Road	Asphalt or Concrete, per County Standards
Pedestrian Trail on Country Roads	Asphalt, Decomposed Granite or Compacted Earth, per County Parks Standards
Core Area Sidewalks	Asphalt Strips, Decomposed Granite, Concrete, Brick, Cobblestone, or Other Acceptable Materials
Equestrian Trails	Compacted Earth or Chipped Bark or Decomposed Granite, per County Parks Standards
Road Shoulder	Compacted Decomposed Granite or Aggregate Base, per County Standards
Core Area Pathways	Asphalt, Concrete, Brick, Cobblestone, etc. — Also, Rolled Curbs

E. Equestrian Trail At Road Crossing



## F. Southwest San Martin Area Development Guidelines

The following development guidelines focus and supplement adopted General Plan policies as they are to be applied to the southwest San Martin area bounded by Highland Avenue to the north, Fitzgerald Avenue to the south, Santa Teresa Expressway to the west and Monterey Highway to the east. These guidelines, along with General Plan policies, are to be used in reviewing and conditioning development proposals.

- ▲ In conjunction with the County Transportation Agency road standards and regulations, road alignments should enhance the rural appearance of the area.
- ▲ New streets should be planned to minimize additional street intersections, particularly with Monterey Highway and Santa Teresa Boulevard, so as to minimize traffic conflicts on these major roads and maintain their maximum traffic capacity. Consideration should be given to a street providing access to Monterey Highway in the vicinity of Church Avenue.

- ▲ New streets and driveways should be planned to minimize the total amount of impermeable surface and yet retain a rural atmosphere as opposed to the straight, gridline type of alignments.
- ▲ The road circulation network should be designed to support the continuity of the rural land use pattern, and limit the amount and speed of through traffic.
- ▲ Design of roads and roadsides should provide for drainage of the road, only.

### **Major Streets: Monterey Highway, Santa Teresa Boulevard, Fitzgerald Avenue**

- ▲ Development along these major streets should provide appropriate setbacks to provide for visual and noise buffering.
- ▲ Landscaping and tree planting along the roadways should screen suburban and rural development while providing for long-range open space views across agricultural lands to the hillsides.

### **Secondary Streets: Highland Avenue, Turlock Avenue, two proposed new north-south streets**

- ▲ Where feasible, new development, road construction and reconstruction activity should maintain the scenic, rural character of these roads with tree-lined streets and appropriate drainage for the road, only.

### **Minor Streets: Carls Court, Neva Lane (not yet constructed) and other future streets**

- ▲ Maintain and enhance the strong sense of the rural landscape throughout the area through landscape requirements consistent with traditional and/or naturalized vegetation.

### **Pathways**

- ▲ Where feasible, all roads should incorporate safe pedestrian, roadside bicycle and/or equestrian pathways such that scenic and riparian values are maintained.

## V. RURAL RESIDENTIAL SUBDIVISION/CLUSTERING GUIDELINES

### A. Introduction

This section of the Plan will develop and illustrate *alternative design concepts* for the clustering and design of rural residential uses. In San Martin, like so many other rural communities, the generally accepted and appropriate approach to rural zoning is to create high density centers. These are then surrounded by assorted large lot residential districts, typically ranging from one to five acres in size. In San Martin, the size of the parcel is dictated by limitations on the soil to handle septic systems. A five acre minimum has been established.

In San Martin, there appears to be a consensus that low density zoning alone does not ensure the protection of the environment and the preservation of rural character.

Development of individual homes on five acre lots will not result in the large open spaces more typical of rural areas.

The expansive open spaces and fields of the valley environment in San Martin are key elements in the rural viewscape. While future residential development will claim many of these high valued areas, the impact of their loss may likely be lessened by residential clustering that preserves some large areas of open space.

Many communities like San Martin have attempted to mitigate these farm field subdivisions by adopting cluster option ordinances that allow individual lots to be smaller than the minimum allowed under conventional subdivision requirements.

Many applicants, however, wind up engineering a conventional plan because of the increased time it takes to get a cluster approval or because having one's own large yard is a desirable asset in a rural community. While there is agreement that clustering is a good thing, the complexities involved may discourage its use.

While it is relatively easy to agree on the number of permissible units, the physical arrangement of these units dictated by a rigid ordinance, often runs counter to the basic objective of preserving San Martin's rural character.

As long as the maximum number of lots or units has been agreed upon, the challenge is to arrange them in the best possible manner while

respecting a site's unique character. A major clustering objective should be to minimize the total amount of site disturbance. Even very low density development can result in considerable site disruption - long, wide roads, large water detention facilities and excessive building envelopes all encourage unnecessary disturbance.

The following recommended alternative guidelines depict *potential* solutions for clustering, orientation of buildings, arrangement of driveways, landscape, fences and other design elements found in the rural residential environment. They are in no way meant to dictate specific design solutions. Each subdivision is unique and must deal with a complicated mixture of design issues.

**B. Alternative Implementation Techniques  
To Improve Rural Atmosphere**

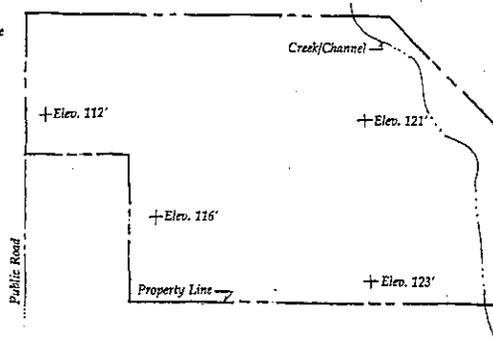
**TABLE 2**

**Development Type: Single-Family  
Development Landform: Farm Fields**

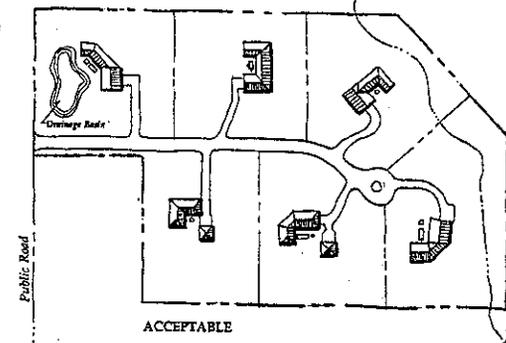
<b>Goals</b>	<b>Alternative Implementation Techniques To Improve Rural Atmosphere</b>
Minimize Visual Impact	<ol style="list-style-type: none"> <li>1) Structures should avoid being placed in the middle of open fields. Locate residences adjacent to any existing tree lines or wooded field edges whenever possible.</li> <li>2) All residences and other structures should be kept at least 150' from the edge of any stream.</li> <li>3) Residences should not directly front on public streets whenever possible.</li> </ol>
Retain Rural Features	<ol style="list-style-type: none"> <li>1) Existing farm roads should be incorporated into subdivision design whenever possible.</li> <li>2) Existing trees, ponds and wells should be preserved where feasible.</li> <li>3) Existing agricultural structures such as barns, tank houses and windmills should be preserved where feasible.</li> </ol>
Minimize Site Disturbance	<ol style="list-style-type: none"> <li>1) Private subdivision roads should follow existing contours or existing routes whenever possible.</li> <li>2) Disturbance for the construction of roads, basins and other improvements should be kept at a minimum.</li> <li>3) Disturbance on individual lots should be limited.</li> </ol>
Minimize Off-Site Flooding	<ol style="list-style-type: none"> <li>1) Where feasible, it is encouraged that sites be graded to create shallow "holding points" for on-site water retention/detention.</li> <li>2) Minimize the amount of impervious surfaces on the site.</li> <li>3) Sheet flow water directed toward other properties is strongly discouraged.</li> </ol>

### C. 30 Acre Conventional Clustering Scenarios

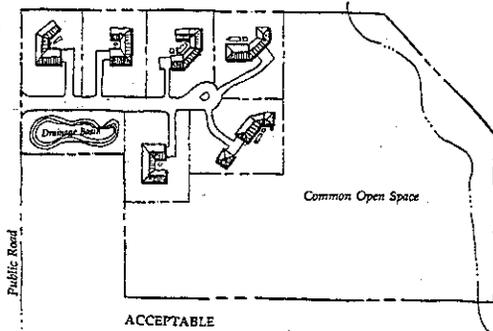
**EXISTING CONDITION:**  
30 acre agricultural farm fields. Five acre zoning.



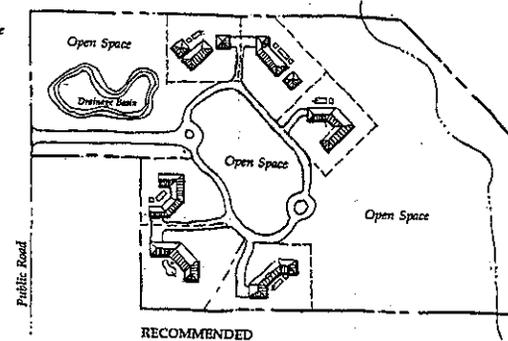
**CONVENTIONAL SUBDIVISION:**  
Six (6), five acre parcels. 30 foot wide cul-de-sac.



**CONVENTIONAL CLUSTER:**  
Six (6), Two acre parcels. 30 foot wide cul-de-sac. 17 acres of common open space.



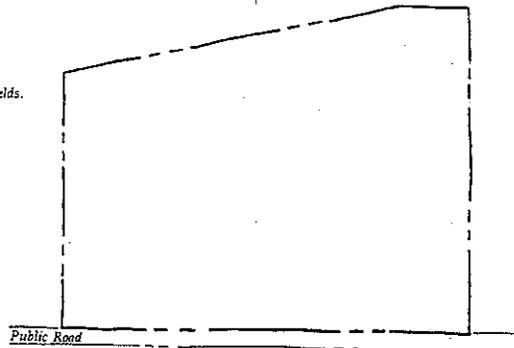
**CREATIVE CLUSTER:**  
Six (6), one acre parcels. 24 foot wide common driveway (private).



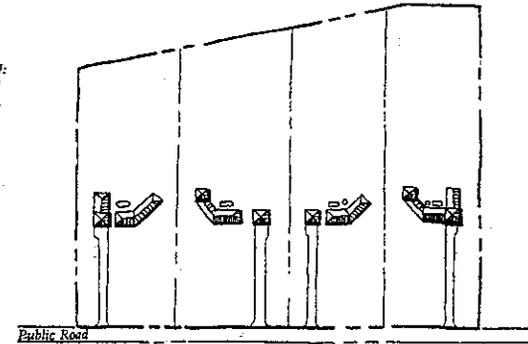
*Note: The clustering scenarios illustrated above should be considered as alternatives to existing County Standards designed to promote the rural lifestyle and image of San Martin.*

## D. 5 Acre Parcelization / Driveway Alternatives

**EXISTING CONDITION:**  
20 acre agricultural farm fields.  
Relatively flat. No vegetation.

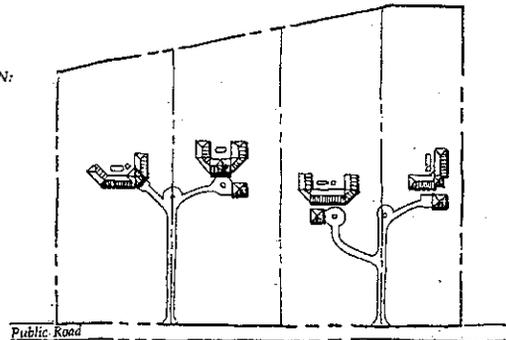


**CONVENTIONAL SUBDIVISION:**  
Four (4), five acre parcel. Individual driveways with access to public road.  
Repetitive front setbacks.



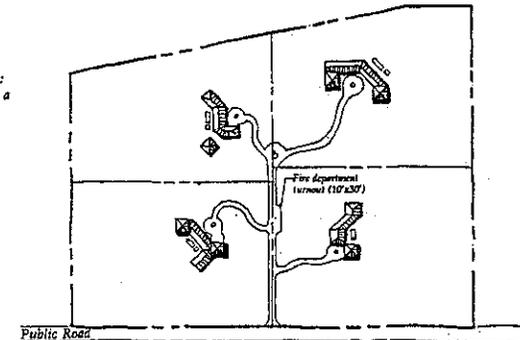
NOT RECOMMENDED

**CONVENTIONAL SUBDIVISION:**  
Four (4), five acre parcels. Use of common driveways. Variable front setbacks.



ACCEPTABLE

**CONVENTIONAL SUBDIVISION:**  
Four (4), five acre parcels. Use of a single common driveway.

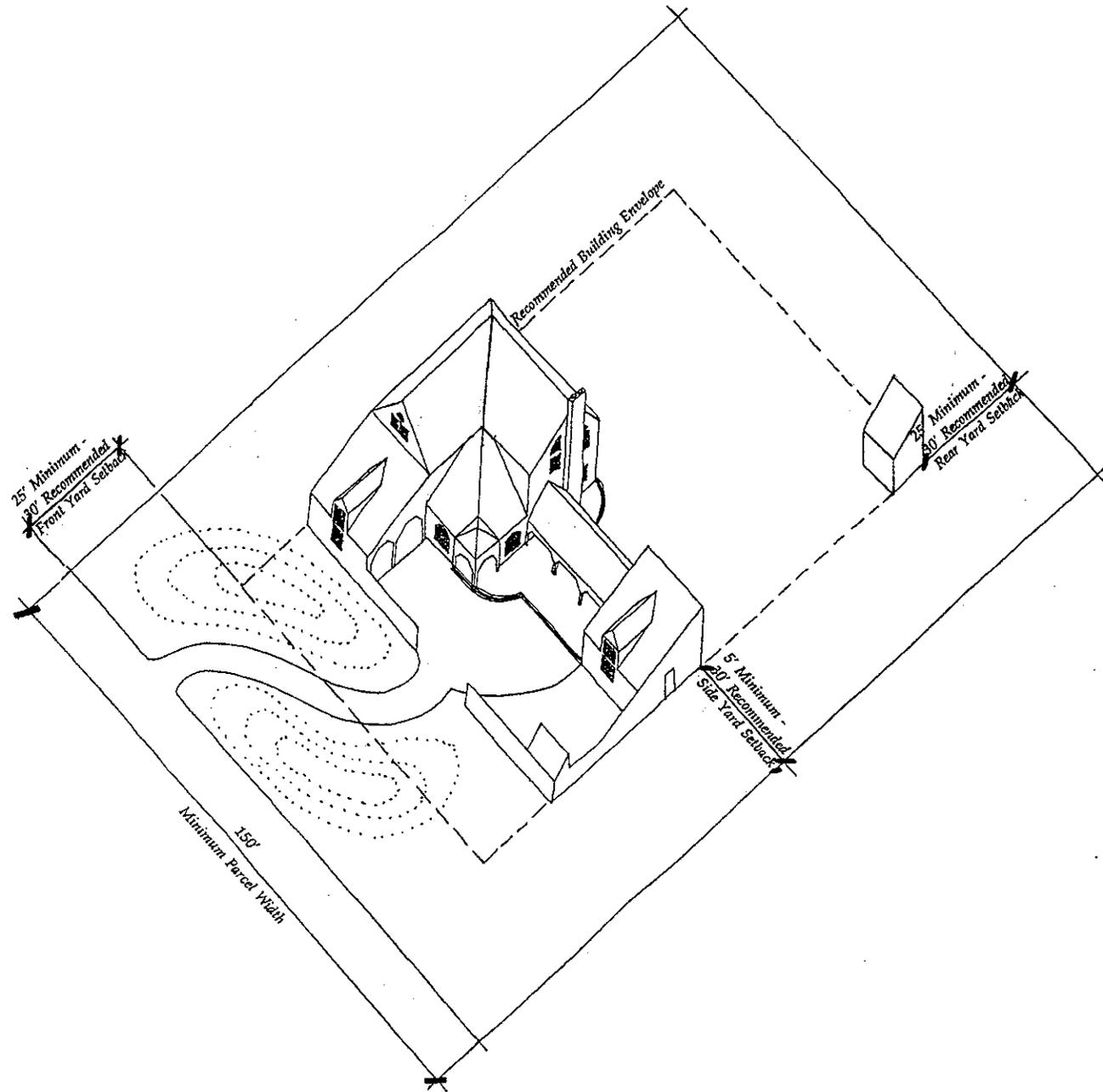


PREFERRED

*Note: The driveway concepts illustrated above should be considered as alternatives to existing County Standards designed to promote the rural lifestyle and image of San Martin. These solutions may not be advisable on some unusual parcel shapes.*

## E. 1 Acre Building Envelope

- ▲ *Minimum Parcel Size:*  
40,000 square feet.
- ▲ *Maximum Building Coverage:*  
20 percent.
- ▲ *Air conditioning units should be ground mounted, located in the rear or side yard, sound attenuated and screened from public view.*

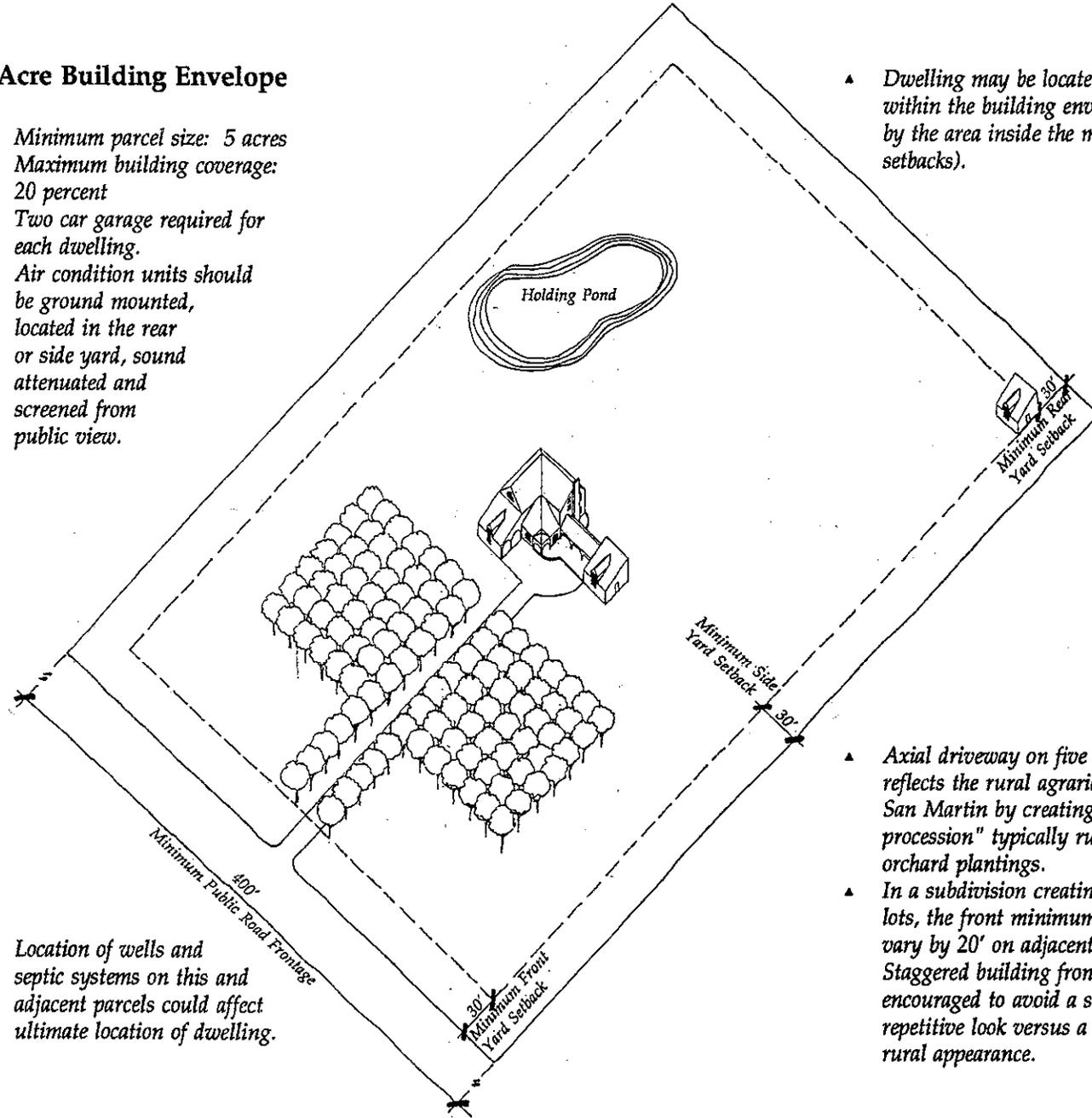


- ▲ *Location of wells and septic systems on this and adjacent parcels could affect ultimate location of dwelling.*
- ▲ *Dwellings may be located anywhere within the building envelope (created by the area inside the minimum setbacks).*
- ▲ *Serpentine driveway and earth berms can effectively screen front yard parking compound.*

**F. 5 Acre Building Envelope**

- ▲ *Minimum parcel size: 5 acres*
- ▲ *Maximum building coverage: 20 percent*
- ▲ *Two car garage required for each dwelling.*
- ▲ *Air condition units should be ground mounted, located in the rear or side yard, sound attenuated and screened from public view.*

- ▲ *Dwelling may be located anywhere within the building envelope (created by the area inside the minimum setbacks).*



- ▲ *Location of wells and septic systems on this and adjacent parcels could affect ultimate location of dwelling.*

- ▲ *Axial driveway on five acre parcel reflects the rural agrarian heritage of San Martin by creating a "grand procession" typically running through orchard plantings.*
- ▲ *In a subdivision creating more than 3 lots, the front minimum setback should vary by 20' on adjacent properties. Staggered building front setbacks are encouraged to avoid a static "planned" repetitive look versus a more organic rural appearance.*

## G. Rural Residential Character Designs

The following architectural ideas are intended to promote the rural lifestyle of San Martin. While these architectural depictions are not guidelines, they attempt to establish an architectural palette consistent with San Martin's "country" image.

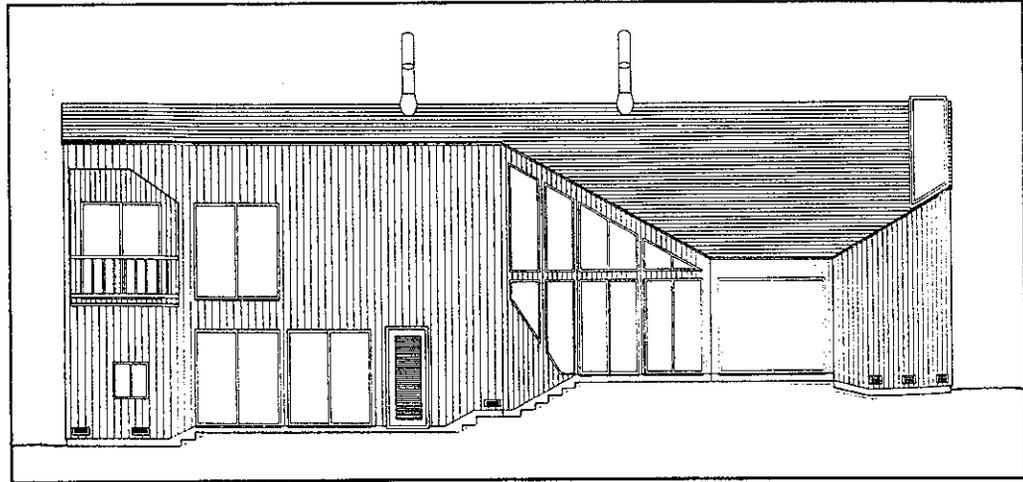
There is no specific "style" being promoted for the rural residential structures in San Martin. The focus is on constructing a high quality "rural" residence. The "rural" qualities are achieved through the use of natural building materials, colors and textures, such as rough wood siding and/or stonework.

There is no desire to *dictate* building types in San Martin's rural residential areas, there are clearly good choices and not so good choices when it comes to selecting a style which "fits" with the rural atmosphere. However, part of the rural lifestyle is being able to construct your dwelling anyway you wish to without a *required* architectural style. Design of our personal residence is one of our most personal and treasured values.

*Please note that the following illustrations are provided only to illustrate prototypical architectural styles which may be appropriate for San Martin. While not entirely inclusive, the illustrations are designed to promote the rural agrarian image of San Martin by providing an initial architectural vocabulary which complements its country setting. The architectural prototypes illustrated are provided to illustrate "idealized" dwelling massing, proportion, scale, building materials and ornamentation. While the examples illustrated are primarily two stories in height, due to their ability to convey a variety of architectural information at-a-glance, more modest single story versions of these architectural styles are also quite appropriate.*

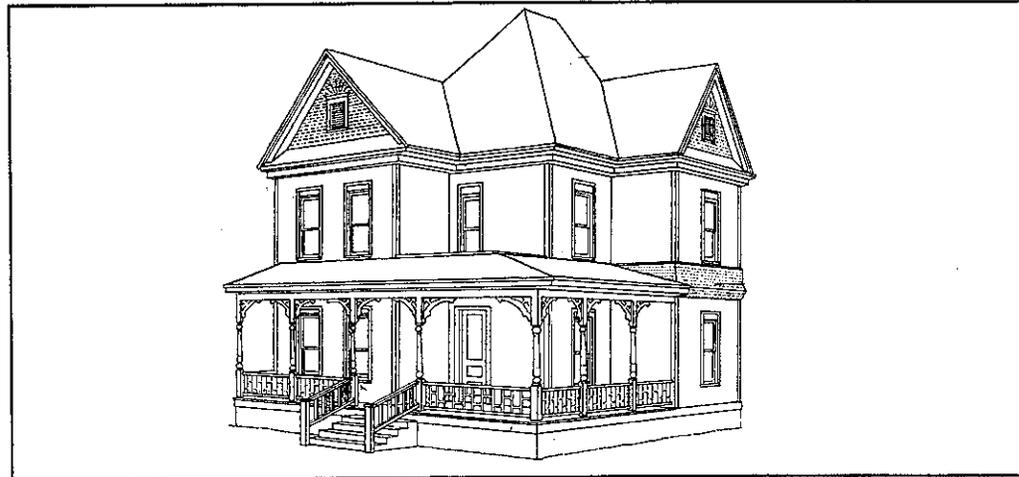
### Contemporary Rural

- ▲ *Modern forms with natural materials*
- ▲ *Primarily wood siding and glass*
- ▲ *View oriented siting*
- ▲ *Metal and masonry highlights*
- ▲ *Typically one or two story*



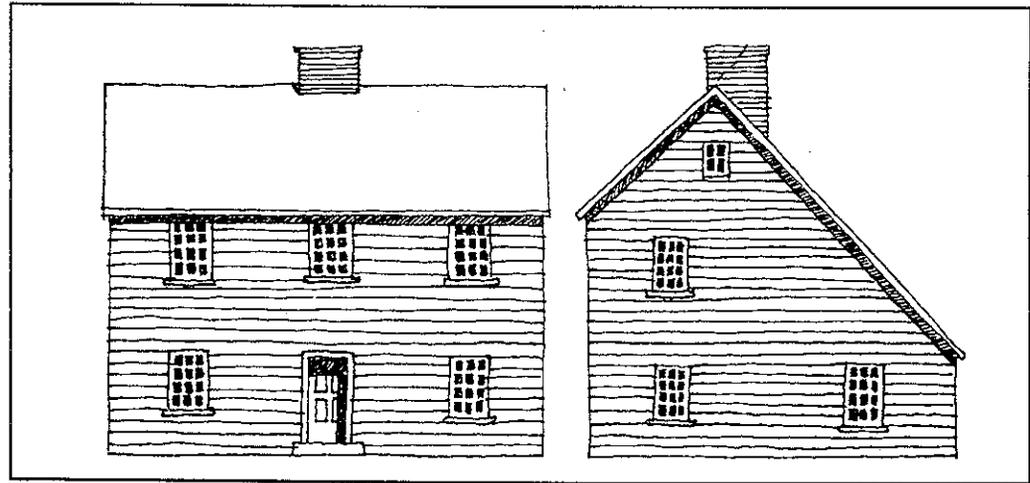
### Victorian Bonanza

- ▲ *Wood frame*
- ▲ *Wrap around open porch*
- ▲ *Period ornate details*
- ▲ *Architectural projections*
- ▲ *Turrets*
- ▲ *Stained glass treatments*
- ▲ *Contrasting trim with dark stained horizontal wood siding*
- ▲ *Typically two stories*



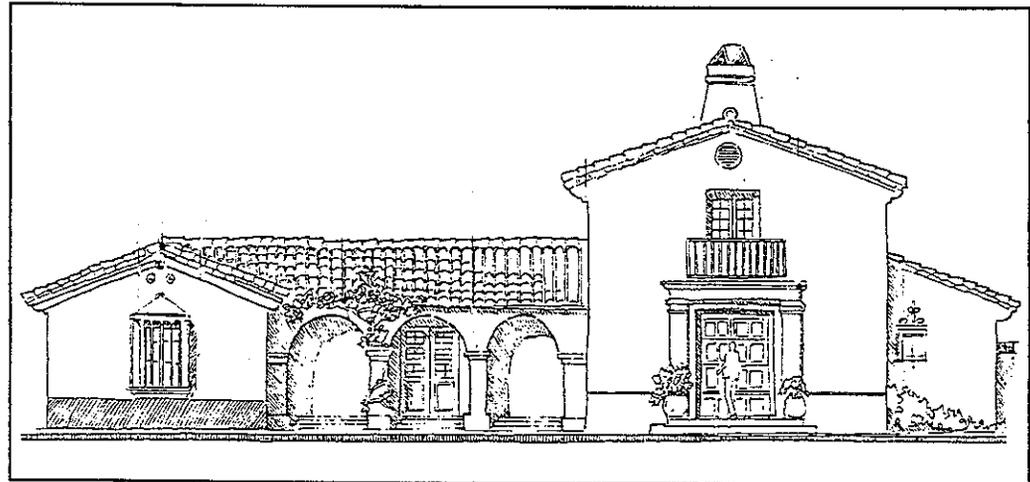
### New England Frame

- ▲ *Horizontal clapboard siding or wood shingles*
- ▲ *Shutters*
- ▲ *Porch*
- ▲ *Edge trim*
- ▲ *Small eaves*
- ▲ *Multi planed roofs*
- ▲ *Typically two stories*
- ▲ *"Salt Box" building mass*
- ▲ *Multi-paned windows*
- ▲ *Gabled roof forms*



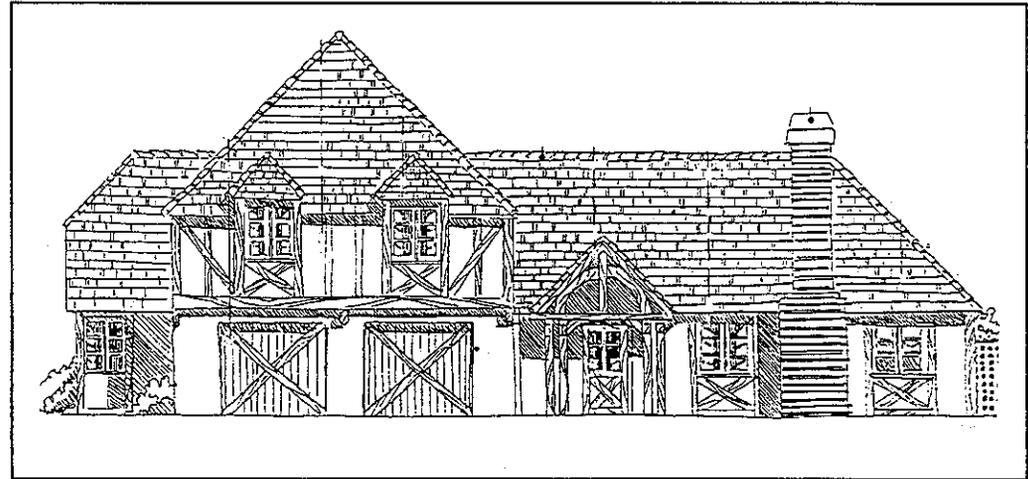
### Spanish Ranch

- ▲ *Combination one and two story mass*
- ▲ *Tile roofs (straight barrel mission)*
- ▲ *Hip and shed roof combinations*
- ▲ *Arcades or colonnades*
- ▲ *Massive wall appearance*
- ▲ *Pedimented entry with shallow second floor balcony*
- ▲ *Cornice return along roof rake*
- ▲ *Casement windows*
- ▲ *Smooth stucco with round corners*
- ▲ *Tile roof vents and decorative window grates*



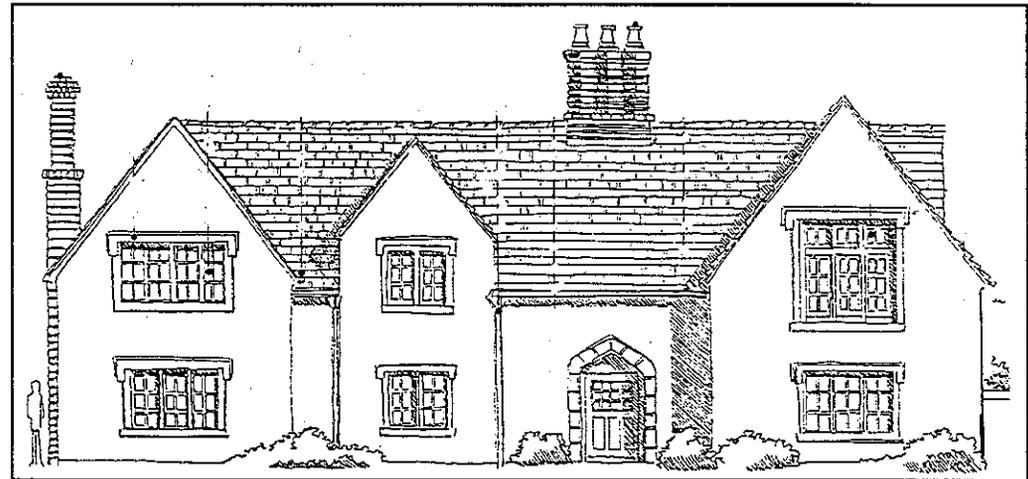
### French Country

- ▲ *Farmhouse origins*
- ▲ *Varied hip roof forms which are steeply pitched*
- ▲ *Shingle roofs with barrel tile ridge*
- ▲ *Brick chimney*
- ▲ *Half timbering*
- ▲ *Walls of stucco or brick*
- ▲ *Earth tones*
- ▲ *Casement windows*
- ▲ *Hip gable dormers*
- ▲ *Entry porch with arched truss*



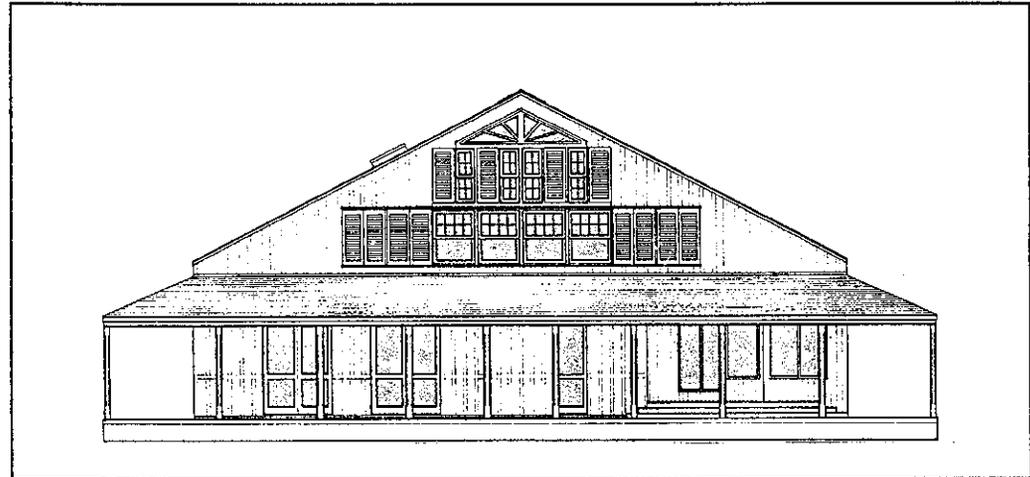
### English Country

- ▲ *Smooth stucco walls*
- ▲ *Wood shake or slate roofs*
- ▲ *Ornate brick chimney*
- ▲ *Steeply pitched gable or hip roofs*
- ▲ *Upper floor may be slightly cantilevered over the first story*
- ▲ *Copper gutter down spouts*
- ▲ *Curved plaster soffit at eave*
- ▲ *Recessed entry*
- ▲ *Casement windows sometimes with transoms above*



### Early California Ranch

- ▲ "Rambling" low horizontal profile
- single story
- ▲ Masonry or wood materials
- ▲ Heavy timber accents
- ▲ Trellises
- ▲ Patios and courtyards
- ▲ Outdoor living areas
- ▲ Low pitched shingle roofs
- ▲ Moderate to wide roof overhangs



### Monterey

- ▲ Simple two story volume
- ▲ 4-in-12 pitched roofs
- ▲ Second floor balconies
- ▲ Combines wood with smooth troweled stucco walls that have little or no texture
- ▲ Multi-paned windows
- ▲ Full length windows opening onto balcony
- ▲ Cantilevered second story balcony covered by principal roof
- ▲ Wood shingle or ceramic tile roofs



## H. Southwest San Martin Area Development Guidelines

The following development guidelines focus and supplement adopted General Plan policies as they are to be applied to the southwest San Martin area bounded by Highland Avenue to the north, Fitzgerald Avenue to the south, Santa Teresa Expressway to the west and Monterey Highway to the east. These guidelines, along with General Plan policies are to be used in reviewing and conditioning development proposals.

- ▲ Preserve and enhance the rural character of the San Martin community where new development occurs. This may be achieved through subdivisions or land development approvals.
- ▲ In new subdivisions, promote flexibility of design, incorporate open space where possible and minimize the length and number of new roads/streets and driveways. Some examples include larger building setbacks or subdivision development with shared open space land and shared access roads/ driveways. Cluster design should be considered where appropriate and feasible.
- ▲ Tree planting and landscaping should be promoted to blend new development into the rural setting throughout the area.

## VI. IMPLEMENTATION

### A. Introduction

Many of the ideas presented in the Design Plan and Guidelines cannot be immediately implemented. In order to move forward with these ideas, the Board of Supervisors must approve new policies and set aside funding for work items such as traffic improvements and new construction along roadways. Follow-up will be required in the areas of Circulation, Flood Control and Drainage and Rural Residential Subdivisions. Since guidelines for Non-Residential development were adopted several years ago and are currently in use, no additional implementation measures are required.

The following sections discuss the primary steps necessary to implement the ideas presented in the Design Plan.

### B. Circulation

1. Request that the Board of Supervisors consider adoption of a "country road" standard for specified roads in San Martin.

2. The Board will refer consideration of this issue to the Department of Roads and Airports for the studies and evaluation needed to determine the feasibility and timing of the following actions:

- ▲ Improving specified roads to create travel lanes wide enough (16') to be shared by cars and bicycles, and to provide a shoulder (3') on at least one side.
- ▲ Evaluation of costs and timing associated with roadway improvements, including adding new pavement or construction of shoulders.
- ▲ Evaluate possibility of connecting existing drainage ditches by constructing new ones in those areas where they don't currently exist.

- ▲ Widening existing road drainage ditches to contain more runoff during rainy seasons and to serve as a pathway/horse trail during dry periods.
  - ▲ Evaluation of construction costs for ditch widening as well as on-going responsibility for maintenance (weed control and debris removal).
  - ▲ Evaluation of costs and location of any new landscaping along roadways.
  - ▲ Determination of need for widening selected bridges to accommodate a pedestrian/equestrian pathway.
  - ▲ Evaluation of need to widen or relocate any existing culverts.
3. Refer to Parks Department's Trails and Pathways Committee in coordination with the Department of Roads and Airports regarding:

- ▲ Location of a pedestrian/ bike path through San Martin that would run both parallel to Highway 101 and across the valley connecting through the village core, with the primary emphasis being locating the path within the existing right-of-way.
- ▲ Construction and maintenance costs of such a pathway system.

### C. Drainage And Flooding

Ask the Board of Supervisors to initiate a planning process in conjunction with the Santa Clara Valley Water District which implements existing General Plan policies and the recommendations of this study in the following areas:

- ▲ Prevent significant alteration of the channels of Llagas Creek and its tributaries.
- ▲ Support current policy of setbacks from riparian areas and require that existing riparian vegetation be preserved.
- ▲ Establish setbacks from known floodways.
- ▲ Seek dedication of creek or tributary easements as a

condition of new development, if not required by the SCVWD.

- ▲ Require new parking lots to be designed so that they retain storm water for eventual percolation.
- ▲ Use of SCVWD maintenance roads along Llagas Creek as public pathways.
- ▲ Responsibility for and funding sources for additional trees and riparian vegetation along the banks of Llagas Creek.
- ▲ Potential for widening the channel of Llagas Creek to carry 100 year flood waters through San Martin. (Note: May involve study of how such widening would impact areas downstream from San Martin.)
- ▲ Funding sources and timing for potential channel improvements.
- ▲ Potential for widening any of the existing small channels draining into Llagas Creek in order to improve drainage in the San Martin area.
- ▲ Coordination with the Department of Roads and Airports in determining size and shape of roadside ditches and

the impacts of any increased carrying capacity on the outfall points along Llagas Creek.

- ▲ Further investigation of on-site drainage techniques, in conjunction with the County Surveyor's Office, to insure that future development does not further impact the existing drainage problem.
- ▲ Establishing appropriate setbacks for new construction from known floodways.
- ▲ Establishing policies to insure that Llagas Creek is not culverted, but remains natural.
- ▲ Periodic clean-up of creek beds, drainage swales and culverts to allow for free flow of water and channels which carry the maximum volume possible.
- ▲ Promotion of drainage retention/detention basins as a means to keep drainage resulting from new development on site rather than contributing to channels which may have already reached capacity. Guidelines for such facilities are found on Page III-9 of the San Martin Plan.

**D. Rural Residential  
Subdivision And Clustering**

Ask the Board of Supervisors to adopt the implementation techniques outlined in Table 1 (page V-3) as part of the Rural Residential Guidelines. The technique calling for on-site retention/detention basins, however, should not be approved without consultation from the SCVWD and the County Surveyor's Office, as noted above.